

**TOWN OF SULLIVAN'S ISLAND
DESIGN REVIEW BOARD**

**REGULAR MEETING MINUTES
Wednesday, February 21, 2024**

A regular meeting of the Town of Sullivan's Island Design Review Board was held at 4:00 p.m. at Town Hall. All requirements of the Freedom of Information Act were verified to have been satisfied. Present were Board members Beverly Bohan, Bunky Wichmann, Heather Wilson, Phil Clarke, and Ron Coish.

Town Council Members present: Mr. Scott Miliment and Mr. Gary Visser.

Staff Members present: Charles Drayton, Planning and Zoning Director, Max Wurthmann, Building Official, and Jessi Gress, Business Licensing and Building Permit Technician.

Media present: No members of the media were present.

Members of the public: Ms. Cindy Ewing, property owner of 2514 I' On Avenue, Mr. David Russell, property owner at 1102 Osceola, Mr. Ian Waterson, property owner of 2630 I' On Avenue, Mr. Randy Wilgis property owner at 2202 Ion, Ms. Kimberly Brown, property owner at 2118 Pettigrew, Ms. Barbra Flaherty, property owner at 2208 Atlantic, and Mr. Dane Derbyshire, property owner at 2118 I' On Avenue.

CALL TO ORDER: Ms. Bohan called the meeting to order at 4:00 p.m. and stated that the press and public were duly notified pursuant to State Law and a quorum of Board Members were present.

- I. **EXECUTIVE SESSION: TO DISCUSS THE POWERS OF THE BOARD TO PLACE CONDITIONS ON APPROVALS:** Mr. Wichmann made a motion to move to executive session. Mr. Coish seconded this motion. All were in favor. None opposed. Motion passed unanimously. Ms. Bohan brought the meeting back to order at 4:28 p.m.
- II. **APPROVAL OF THE JANUARY 17, 2024 MINUTES:** Mr. Wichmann made a motion to approve the January 17, 2024 Design Review Board Meeting Minutes. Mr. Coish seconded this motion. All were in favor. None opposed. Motion passed unanimously.
- III. **PUBLIC COMMENT:** No public comment was made.
- IV. **PROCESS FOR DESIGN REVIEW:** Ms. Bohan reviewed the meeting process for the Design Review Board which is as follows:
 - Statement of matters to be heard (Chair announcement)

- Town staff presentation (5-minute limit)
- Presentation by applicant (10-minute limit)
- Town staff final statement (if needed)
- Board Q & A (may occur at any point during hearing)
- Public comment closed
- Board deliberation and vote

V. HISTORIC DESIGN REVIEWS:

1501 Middle Street: Brooke Gerbacht, of Herlong Architects, requested final approval to renovate this Traditional Island Resource home and construct a two-story addition with requests for historic exemptions to increase the principal building square footage, coverage area, and impervious coverage area, along with requests to reduce the side setback, the second story side façade setback, and the accessory structure setback (Supporting documentation) (523-08-00-018).

Mr. Drayton stated that this is the DRB's third review of this project; the Board granted preliminary approval to the project at last month's meeting, with a request for the applicant to consider options for further reducing the massing by making the rear addition a story and a half rather than two stories and other considerations to adjust the rooflines. In response, the applicant has lowered the ridge of the addition on the left side of the property by adding dormers to hide some of the square footage within the rooflines; they have also switched to a shed roof over the proposed front porch of the addition which cleaned up the multiple gables facing Middle Street. The subtle changes to the design address the concerns from the Board, but the outcomes created no changes to the relief requests.

Mr. Drayton stated that staff recommended that the Board grant final approval to the addition plans if the Board finds the addition conforms with the Standards for Neighborhood Compatibility and the Secretary of the Interior Standards for Historic Properties.

Ms. Gerbacht presented her application to the Board.

No public comment was made.

Ms. Wilson stated that she was not in favor of the proposed garage height and thought that the garage massing should be reduced. Mr. Clarke agreed.

The Board agreed with Ms. Wilson's comments and requested that the massing of the garage be returned to the original height it has been in previous applications.

Ms. Wilson made a motion to grant final approval for the application presented provided that the applicant reduce the garage ridge height back to its original design. Mr. Wichmann seconded this motion. All were in favor. None opposed. Motion passed unanimously.

2513 I 'On Avenue (Application): Eddie Fava, of E.E. Fava Architects, requests final approval for the proposed renovation work to this Sullivan's Island Landmark property, with historic exemption requests for additional principal building square footage and principal building coverage area (PIN# 529-10-00-037).

Mr. Drayton stated that this is the DRB's second review of this project; at the December 2023 meeting the Board gave a conceptual review to the project and was generally in favor of the applicant's proposed delicate touch for the renovations of this important historic structure. As previously noted, the façade changes along Atlantic Avenue would only affect non-historic additions and alterations to the property, and on the I 'On Avenue side of the property, there are 3 minor additions: the two corridors are within the recessed courtyards (over 80 feet from the street and 40 feet recessed from the rear façade of the historic property), and the dormer addition on the second floor is recessed even further. The only suggestions from the Board were to study the floor-to-ceiling windows in the additional corridors and to add more historic pictures for reference. The applicant has revised the windows on the additions by reducing the window heights slightly and going from 8-panes (matching the historic windows under the porch) to a 6-pane pattern, which differentiates the addition from the historic. There are also 2 accessory structures located on the I 'On Avenue side of the property; the applicant is also requesting to update the fenestrations in these 2 buildings and to rebuild a longer and wider front porch onto the "Summer House"; the porch on the "Summer House" is currently just a covered landing, but the proposed design stretches the porch to cover most of the cottages front façade, and the porch appears to be about 50% deeper. Neither accessory structure is historic.

Mr. Drayton stated that staff recommended final approval if the Board finds the applicant's historical analysis of the structure, the delicate touch, and modest requests meet the Standards for Neighborhood Compatibility.

Mr. Fava presented his application to the Board.

Ms. Bohan stated that town staff received two letters in regards to this application (Exhibit 1 and 2).

Ms. Cindy Ewing, property owner of 2514 I 'On Avenue, submitted documentation to the Board (Exhibit 3). Ms. Ewing stated that this house is extremely iconic to the neighborhood and requested that the Board take a pause on this application until the applicant does more research to preserve the nature of the historic home.

Mr. Scott Milliment, property owner of 2902 Brownell Avenue, stated that this property is a historical icon and asked that the Board consider the historical nature of this home before granting final approval.

Ms. Bohan referred to the Standards of Rehabilitations items 1-9 specifically items 1,2, 3, 4, 5, 6, and 9 and asked the Board to consider these standards when making a decision regarding this property. Ms. Bohan requested that the Board consider deferring this application until the

applicant provides more information on the historical structure and the Board conducts a walk through of the site.

The Board was in favor of Ms. Bohan's suggestions.

Mr. Wichmann made a motion to defer the application presented providing time for a site visit and a thorough review of the home to be conducted. Ms. Wilson seconded this motion. All were in favor. None opposed. Motion passed unanimously.

1118 Osceola Avenue: Eddie Fava, of E.E. Fava Architects, requested final approval to restore and relocate the house on this Sullivan's Island Landmark property and to relocate and connect another home, from the adjacent parcel within the Stella Maris Church campus, to the rear of the historic house on the property (523-07-00-083 & -084).

Mr. Drayton stated that this is the Board's second review of this project; at the first meeting in December 2023 the Board granted preliminary approval to the project. With the vote for preliminary approval the Board requested that the applicant be prepared, upon return, to detail more of the master plan for the church campus so that the Board can better understand how this approval would fit with the long-term plan, to provide additional historical analysis of the Schill House, and to study the connection between the 2 buildings. The applicant has provided greater detail to the porch hyphen that is proposed to connect the structures and has added details regarding the historic materials in the house. The applicant will discuss any questions regarding the master plan with the Board but does not wish to include those details in the plans, since they are not for review and do not affect the Church's desire to move forward with the scope of the project as requested.

Mr. Drayton stated that staff recommended final approval of the request for the proposed relocations and renovations if the Board finds that the design is in character with the historic property, satisfies the SIS Guidelines for siting "additions" in an historic setting, and meets the Standards for Neighborhood Compatibility.

Mr. Fava presented his application to the Board.

Mr. David Russell, property owner at 1102 Osceola, stated that he wrote a letter to the Board when this application came before the Board in December of 2023 requesting a full master plan of the parking lot. Mr. Russell stated that the applicant hasn't presented any sort of master plan on this application and would ask the Board to deny this application until the full plan is presented.

Mr. Gary Visser, property owner of 2924 Middle Street, stated that this church contributes to the members, residents and the community. After Hurricane Hugo, the Red Cross supplied Stella Maris to cook and provide food to feed the emergency personnel who were on the island. After, the kitchen fed volunteers and the less fortunate for many months. Mr. Visser stated that this project intends to preserve and protect the historic structures as well as tend to the handicapped requirements. Mr. Visser stated that the prospective being shown to the lot boundaries is a significant amount of attention to coverage.

Mr. Fava responded by stating that they are just requesting the work to be done on the structures on this property. Mr. Fava stated that the parking plan, if there will be one, will have to come back to the Design Review Board for review and at that time, the Board and public can have a discussion but, in the meantime, the requests for the structures are the Boards concern at this time.

Father McInerny stated that he is asking the Board to review and consider the application regarding the structures on the lot as the parking plan is not final at this time and is not being requested at this time.

Mr. Drayton stated that the applicant will have to come back to the Design Review Board for review and approval of the parking plan when they have submitted those plans.

The Board agreed that they should review the plans for the structures only at this time.

The Board was in favor of the application presented.

Mr. Wichmann made a motion to grant final approval for the application presented. Ms. Wilson seconded this motion. All were in favor. None opposed. Motion passed unanimously.

Mr. Clark recused himself from the application regarding 2814 Brooks Street (Exhibit 4).

2814 Brooks Street: Bryce Richey, of Clarke Design Group, requested a conceptual review to restore this historic cottage and construct a large addition and detached garage on the property, with a request from the Board for side yard setback relief (529-07-00-046).

Mr. Drayton stated that This is the Board's initial review of this project; the applicant is requesting a conceptual review of the project from the Board. The applicant is proposing to remove some non-historic addition work that was scabbed onto the cottage years ago and to center the cottage along the front setback; it does not appear that the applicant intends to elevate the home beyond its current first floor elevation. Once the cottage is centered on the lot, the applicant is proposing to build a small addition on the right side of the cottage and a large addition on the left side of the cottage that extends to the rear of the lot and proposes a 2-story elevation on the rear. The total square footage of the plan is within the standards of the Town's ordinances, and the only relief the applicant is seeking from the Board is for side setback relief. The requested relief is beyond the scope of what the DRB can grant; the applicant has requested less than 2 feet of relief, but upon further investigation, they are proposing that relief from a 30-ft combined setback, which is the maximum relief the Board could grant, therefore, the actual request is currently for 11 ft 9.5 in. The applicant ascribes the needed relief to a desire to keep construction away from a large, 78 in live oak located at the rear of the property. Staff does concur that the oak is an important feature of the landscape and should be preserved, but it does not appear to cause a hardship on the side setback of the addition on the left side of the cottage. The only other consideration is for the garage on the rear of the property; please ensure that the garage would be both 10 feet from the property line and 30 feet from the edge of pavement.

Mr. Drayton stated that staff recommended the Board consider the Town's Standards for Neighborhood Compatibility and the SIS Guidelines for additions to historic properties and provide feedback to the applicant that will move the project forward towards those standards and guidelines.

Mr. Richey presented his application to the Board.

No public comment was made.

Mr. Wichmann suggest that they keep the historic structure true and try to pull the addition off as much as possible but over all was in favor of the application presented.

The Board agreed with Mr. Wichmann.

Ms. Wilson made a motion to grant preliminary approval for the application presented. Mr. Wichmann seconded this motion. All were in favor. None opposed. Motion passed unanimously.

VI. NON-HISTORIC DESIGN REVIEWS:

2620 Atlantic Avenue: Ross Ritchie, of Loyal Architects, requested preliminary approval to renovate this home and add an elevated pool and accessory structure to the property, with a request to re-orient the home to face I 'On Avenue and requests to increase the height of the accessory structure and increase the principal building square footage (PIN# 529-10-00-030).

Mr. Drayton stated that this is the Board's second review of this project. At the meeting in November 2023 the Board expressed an affinity for the design of the project, but there was considerable concern regarding reorienting the property and allowing a swimming pool to be placed on the Atlantic Avenue frontage. After some deliberation the Board voted to defer recommendation of the project until more could be learned about the neighborhood compatibility of the proposal and if there were any mechanisms to ensure no further development along the Atlantic Avenue frontage if it were reoriented to become the rear frontage. The applicant has provided additional letters of support from the neighborhood for the reorientation; they have provided additional studies of the neighborhood; they have studied the history of reorientation cases on the Island; and they have provided additional; photos, maps and language from their findings to support their request.

Mr. Drayton stated that staff recommended granting preliminary approval of the project if the Board considers plans to be in conformity with the Standards for Neighborhood Compatibility.

Mr. Richie presented his application to the Board.

Ms. Bohan stated that town staff received two letters from the public regarding this application (Exhibit 5 (see exhibit 2 for additional comments)).

Mr. Ian Waterson, property owner of 2630 I' On Avenue, stated that he was in favor of the application presented. Mr. Waterson stated that technically the legal orientation is on I' On, so he supports the location of the pool and looks forward to completion.

Ms. Wilson stated that she finds it compelling that the frontages in this area are on Ion and it makes for a compelling argument. Ms. Wilson asked if the house would be torn down and reconstructed, could they build the new home 25 feet from Atlantic. Mr. Drayton confirmed. Ms. Wilson stated that it's really a gentlemen's agreement for the house to be placed so far back on the lot. With that Ms. Wilson felt that this is a unique situation and would allow for the relocation with the facts presented to the Board.

Ms. Bohan stated that the Waring house was denied back in 2019 for a request to reorient the property to allow a pool on the Atlantic Av side; she stated that Section 21-30 contributes to neighborhood compatibility and the DRB should be cautious about granting approval for the reorientation. Ms. Bohan stated that this sets a precedent for pools located on Atlantic Avenue. Ms. Bohan stated that as the Board, they are charged to hold a duty and there aren't many pools on the Atlantic side. Ms. Bohan felt that the accessory structure is not compatible.

Ms. Wilson stated that the pool is very well hidden, and this property is unique. The applicant is removing two driveways which consist of a lot of asphalt in place of the hardscape that is being requested instead. Ms. Wilson doesn't see a reason to deny this application and the applicant did a really good job with this design.

Ms. Wilson made a motion to grant final approval for the application presented with the condition that no additional accessory structures are placed on the lot. Mr. Clark seconded this motion. Motion passed 4 to 2. Ms. Bohan and Mr. Coish opposed.

VII. COMMERCIAL DESIGN REVIEWS:

2118 I'On Avenue (future 2119 Middle Street): Dane Derbyshire, of D4 Partners, requested a conceptual review of Commercial District special exception for a short-term automobile parking lot in accordance with §21-50 C (4) (PIN# 529-09-00-118)

This is an initial review for the design of a proposed short term auto parking lot to be located on Middle Street in the commercial district. In speaking with the applicant, while the application requests a conceptual review, the applicant is interested in receiving an approval from the Board, and he has provided a complete set of design plans for the Board to consider. Short term auto parking is not a by-right use in the Town's Community Commercial Zoning District; it is permissible through a special exception which was granted by the BZA at its meeting earlier in February. At the BZA meeting that Board expressed concerns about the buffering; they stated that the existing short term auto parking lot does not have enough landscaping, and they expressed concerns about pedestrians leaving their cars at the parking lot and crossing the buffer directly into the Station 22 right of way, which is a congested area without sidewalks. With these concerns in mind the BZA

placed a condition upon their approval that the applicant provide residential fencing along the Station 22 right of way to prevent pedestrian access directly onto that street.

The guidelines for the design of parking lots are found in Section 21-143 of the zoning ordinance, with the specific requirements for parking lots on the split zoned lots in the CCOD 2 District in 21-143 D. (3). These requirements state:

- (a) Direct access provided from parking lot to a public street.
- (b) Parking space: 18 feet x 9 feet; on-street parallel parking space a minimum of twenty (20) feet in length.
- (c) Wheel stops shall be required for all parking area without raised curbing; the vehicle side of the wheel stop shall be no less than eighteen (18) inches from the end of the parking space; where sidewalks or other walkways occur, parked vehicles shall not overhang or extend over the sidewalk. In these parking facilities, wheel stops shall be provided even if the parking facility has curbs.
- (d) Parking driveway aisle for off-street parking: minimum width of eighteen (18) feet for sixty (60) degree angle parking; minimum width of twenty-four (24) feet for ninety (90) degree parking.
- (e) Permeable materials shall be used for on-site parking and driving.
- (f) Access to parking areas shall not be permitted to cross residentially zoned portions of lot.
- (g) Access driveways shall not exceed fourteen (14) feet in width; and,
- (h) Driveways shall be sited to minimize interruption of the continuity of the public sidewalk.

Mr. Drayton stated that staff has reviewed the plan submitted by the applicant and believes that the design meets all the stated requirements, except that the access driveway will be wider than 14 feet, as the BZA affirmed the use of the existing 25.5-foot curb cut. The buffering plan is also under the purview of the DRB, and the buffer requirements are stated in Section 21-143 D. (4). Staff feels the plan addresses all the requirements found therein, so the applicant will only need to show that he is meeting the additional condition placed on the buffering design by the BZA. Additionally, as was discussed in the BZA meeting, staff feels that the buffer tree located to the right for the exiting vehicle should be removed to provide a clear sightline for drivers as they exit the parking lot.

Mr. Drayton stated that staff recommended final approval of the request if the Board finds the applicant has adequately met the requirements found in the zoning ordinance and the additional conditions placed on approval by the BZA.

Mr. Cline submitted revised plans to the Board for review (Exhibit 11). Mr. Cline presented his application to the Board.

Ms. Bohan stated that town staff received five letters from the public regarding this application (Exhibits 6-10).

Mr. Randy Wilgis submitted documentation to the Board (Exhibit 12). Mr. Wilgis stated that he was concerned about this application regarding the living fence. He suggested a solid block fence be in place to block out headlights from the neighboring properties. Mr. Wilgis suggested possibly placing some trees in the middle of the lot and possibly adding a sidewalk for safety.

Ms. Kimberly Brown submitted documentation to the Board (Exhibit 13). Ms. Brown stated that she was concerned about the application and suggested irrigation be implemented to assist with growth along the living fence. Ms. Brown stated that the parking lot lighting was a concern and provided lighting suggestions in her documentation.

Ms. Barbra Flaherty, property owner at 2208 Atlantic Avenue, stated that she was concerned with the application for safety reasons. Ms. Flaherty stated that this is a busy intersection and is concerned for children walking and would like to see a sidewalk be installed around the parking lot.

Mr. Dane Derbyshire, property owner of 2118 I' On Avenue, stated that they have submitted revisions to the application due to the outcome of the BZA and it allows for a living fence which would require irrigation. Mr. Derbyshire stated that he has no problem addressing the lighting to reflect downward.

Ms. Wilson stated that this lot is very similar to the parking lot that was installed a couple of years ago on the next block. Ms. Wilson stated that these parking lots do not have to be so similar. Ms. Wilson stated that the application is not consistent with the presentation and would suggest coming back to the Board with a more consistent plan adding living fence and addressing the lighting concern made by the neighbor.

Mr. Wichmann stated that the applicant should be mindful of the public and should consider adding a sidewalk would encourage a more substantial fence along station 22 and the perimeter of the parking lot.

The Board agreed with Ms. Wilson and Mr. Wichmann.

Ms. Wilson made a motion to defer the application presented. Mr. Wichmann seconded this motion. All were in favor. None opposed. Motion passed unanimously.

VIII. REQUEST FOR REHEARING:

2602 Atlantic Avenue: Brandon Gaskins, attorney at law with Moore & Van Allen, PLLC, is representing Mr. Randy Friedman in his request for a rehearing of his request from December 2023 to remove six (6) historic windows from this Traditional Island Resource property (529-10-00-033).

Mr. Drayton stated that this property is an historic Traditional Island Resource property (Historic Survey Card #440) located just outside of the Atlanticville Local and National Register Historic Districts; the Atlanticville Local Historic District is adjacent to the property on the I'On Avenue side. The historic home was constructed around 1900 and underwent extensive renovations in around

1975 and 2019. In 2019 the 6 windows that are in question were meticulously refurbished and returned to their locations in the home as part of the CoA issued by the DRB in order to permit the renovation and addition work to the home. Staff caught the contractor in the act of replacing those 6 windows without a permit or a CoA in October; a stop work order was issued, but staff did allow the homeowner to keep the new windows that had been illegally put in place to remain until the Board could review the project, so that the owner would not have unsecured openings to his home in the interim. As noted in the contractor's submittal to the DRB, which include the plans submitted to the DRB for approval in 2019, the 6 windows in questions were and are clearly noted with "Repair Existing Window" on each of window locations on the plans.

Mr. Drayton stated that staff recommended the historic windows to be refurbished and placed back on the home in their historic locations in concert with the SIS Guidelines.

Mr. Gaskins presented his application to the Board.

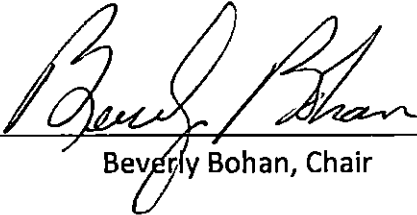
No public comment was made.

Mr. Gaskins stated that the purpose of this request was to receive approval from the Board for a rehearing.

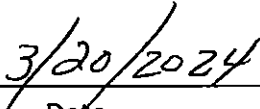
The Board was in favor of granting the rehearing.

Ms. Wilson made a motion to grant approval for the rehearing of the application for 2602 Atlantic Avenue. Mr. Wichmann seconded this motion. All were in favor. None opposed. Motion passed unanimously.

IX. ADJOURN: Mr. Wichmann made a motion to adjourn the meeting at 7:34 p.m. Ms. Wilson seconded this motion. All were in favor. None opposed. Motion passed unanimously.



Beverly Bohan, Chair



Date

Pamela - I am Margaret Bouch. My husband and I have been at 2508 l'On Avenue for almost 30 years. Being in the "historic district" of the island, we feel it is very important to maintain the historic character of the homes in the area. If that character is not maintained, we become just like any other island town along the coast. We lose some of what makes our island so special. We ourselves have gone to great lengths to maintain that historic character both outside and inside our home, and expect others in the historic district to do the same. There are ways to modernize without destroying the historic character. It just may take a little more creativity!

Roy Williams' house at 2513 l'On Avenue is perhaps the crown jewel of the entire historic district. What is being proposed for the l'On Avenue side of that house has us greatly concerned. If those proposed changes to the windows and some of the walls on the l'On side is allowed to proceed, we feel it will ruin what has been a wonderful example of an old Sullivan's Island house. I wish we could attend the Design Review Board meeting tomorrow, but it doesn't look like either one of us can. Please note, though, that we are very displeased with what has been proposed. We hope the Board will not approve it and will ask that the owners come back with ideas for those windows/walls that will maintain the historic character of the house. Perhaps inspiration could be taken from the renovation that was done to the school house at 2302 Middle Street!

I hope that you will pass my comments along to the Design Review Board. Many thanks -

Margaret Bouch (843-834-5570)

To Design Review Board regarding the two houses above. I am unable to attend meeting but feel passionate about these proposed plans:

Comments on 2620 Atlantic:

In our code of design standards it states:

Sec 21-30 Orientation of Principal building

A. Purpose. Principal Buildings with street oriented front doors contribute to the sense of "neighborliness" of the Island; having a consistent front yard orientation, reduces the possibility of accessory structures being located inconsistent with the pattern of development.

B. Design standards.

(1) The Principal Buildings Primary facade (front door) shall be oriented towards a public street, excluding Principal buildings on lots adjacent to the ocean or marsh.

Reversing this home to have the front on Ion would be a precedent setting tragedy.

These homes on the highly historic section of Atlantic should face Atlantic as they have for all of history. Many homeowners have found a way to put pools in front yards or side yards and still not disturb the historic nature of the home. The setbacks of those houses are all in a row in a grand neighborly fashion and should stay that way. The rear side on Ion can still be a lovely area with parking without reorienting the house and address which would be a huge travesty.

Comments on 2513 Ion.

I agree with everything Battery Gadsden said about the windows. The same look can be accomplished with plenty of light with traditional windows (or saving many of the old ones as many residents have done painstakingly.) On a side note, Roy Williams would absolutely want the integrity of the house to be maintained.

Kathy Heller
2320 Ion Ave

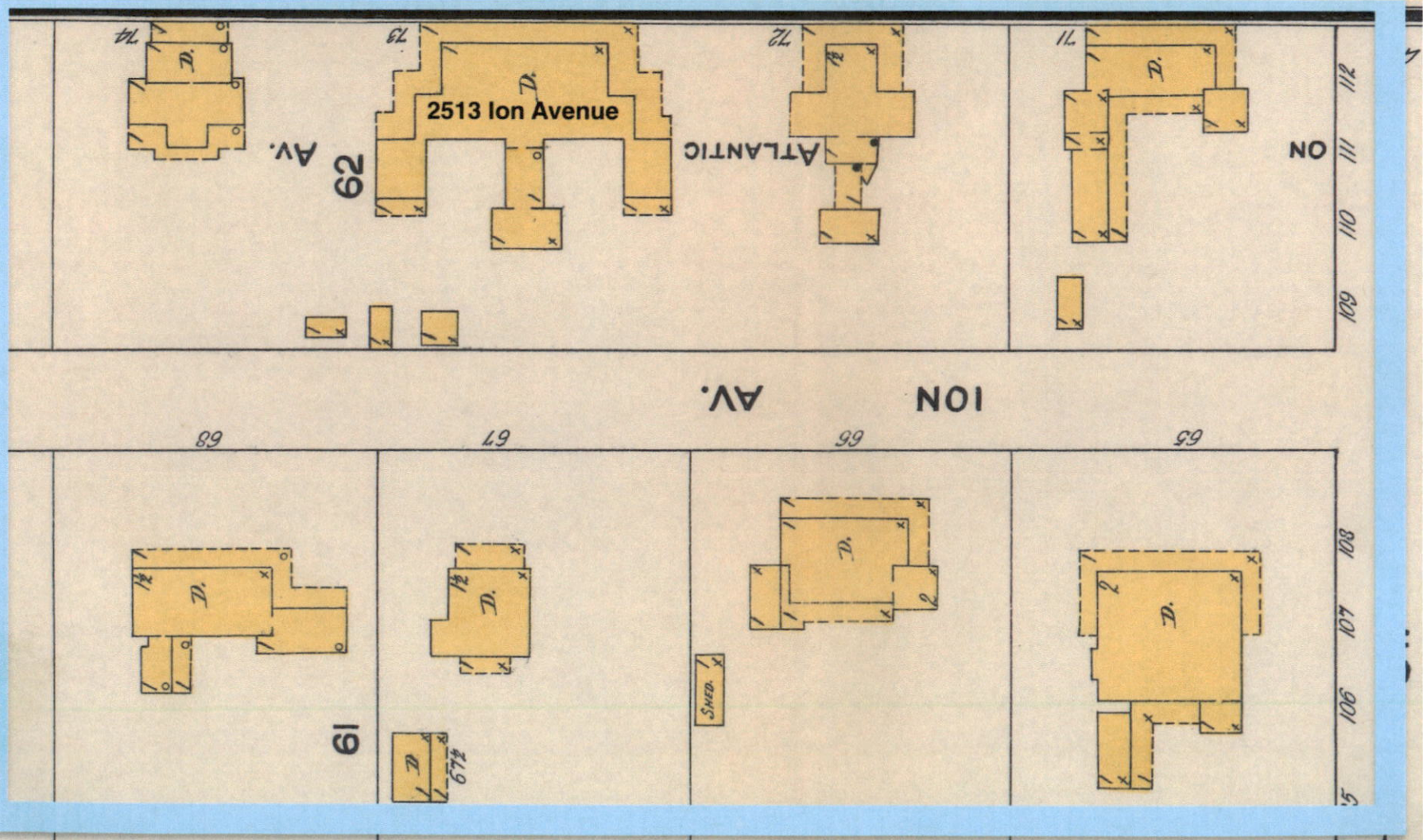
1912 Sanborn Map

Exhibit 3

This area in yellow represents where the beach was 124 years ago when 2513 Ion Avenue was built.

Atlantic Avenue was not paved until the late 1970's so Ion Avenue was the center of this neighborhood and remains the center of our historic neighborhood today.

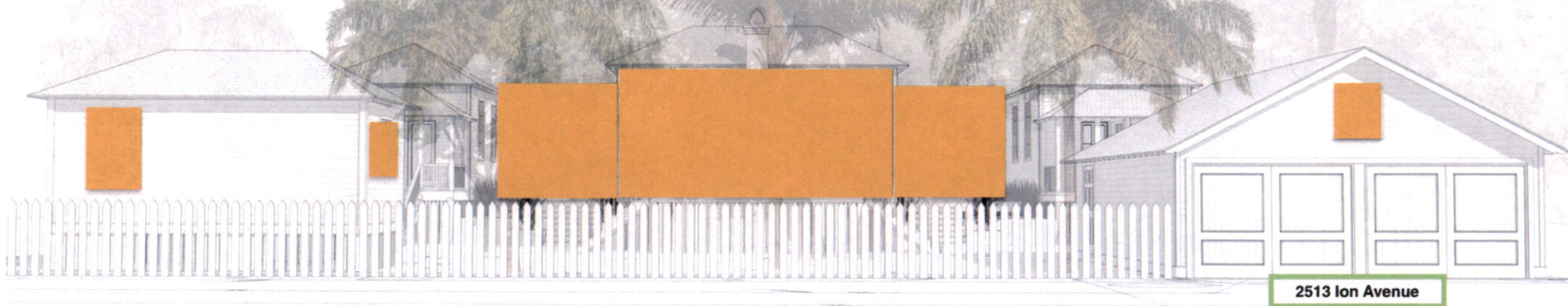
Any changes made to the Ion facades of the Williams house will have a negative effect on the historic character and integrity of the neighborhood.



The orange areas show the extent of the changes requested . This plan will have a SIGNIFICANT and NEGATIVE impact for the house and for our neighborhood.

Approving this plan will be the final tipping point for the integrity of this unique historic neighborhood.

Allowing these changes will set a precedent for future historic homes integrity to be erased.



2513 Ion Avenue



Ion Street Perspective
SCALE

Erasing History by a Thousand Cuts.

6/6 style

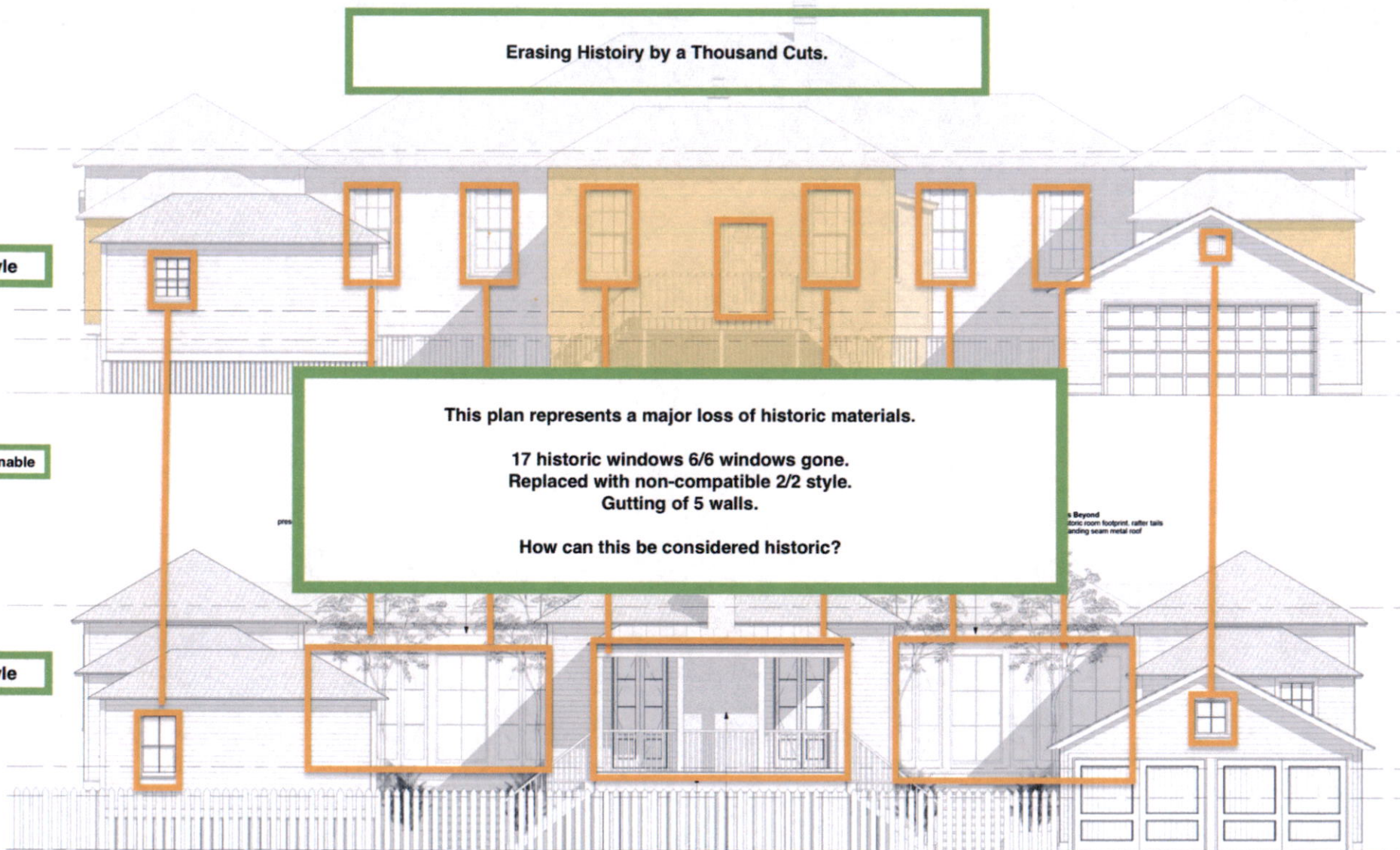
Not interchangeable

2/2 style

This plan represents a major loss of historic materials.

**17 historic windows 6/6 windows gone.
Replaced with non-compatible 2/2 style.
Gutting of 5 walls.**

How can this be considered historic?



see A210 for accessory structure details

New Porch
Standing Seam Metal Roof with painted wood columns and handrail to match existing porch detailing. T&G wood decking with wood steps and treach.

Masonry Chimney
Traditional three coal flue in masonry.

Beyond
stone room footprint, rafter tails
standing seam metal roof

see A210 for accessory structure details

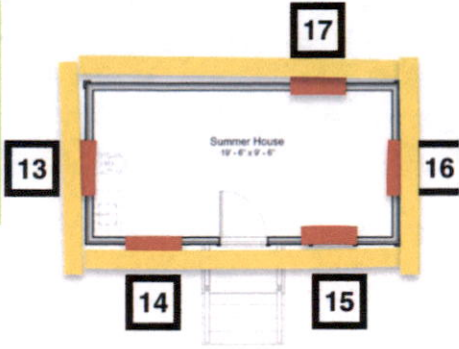


17 = 17 historic windows removed and replaced with incompatible styles

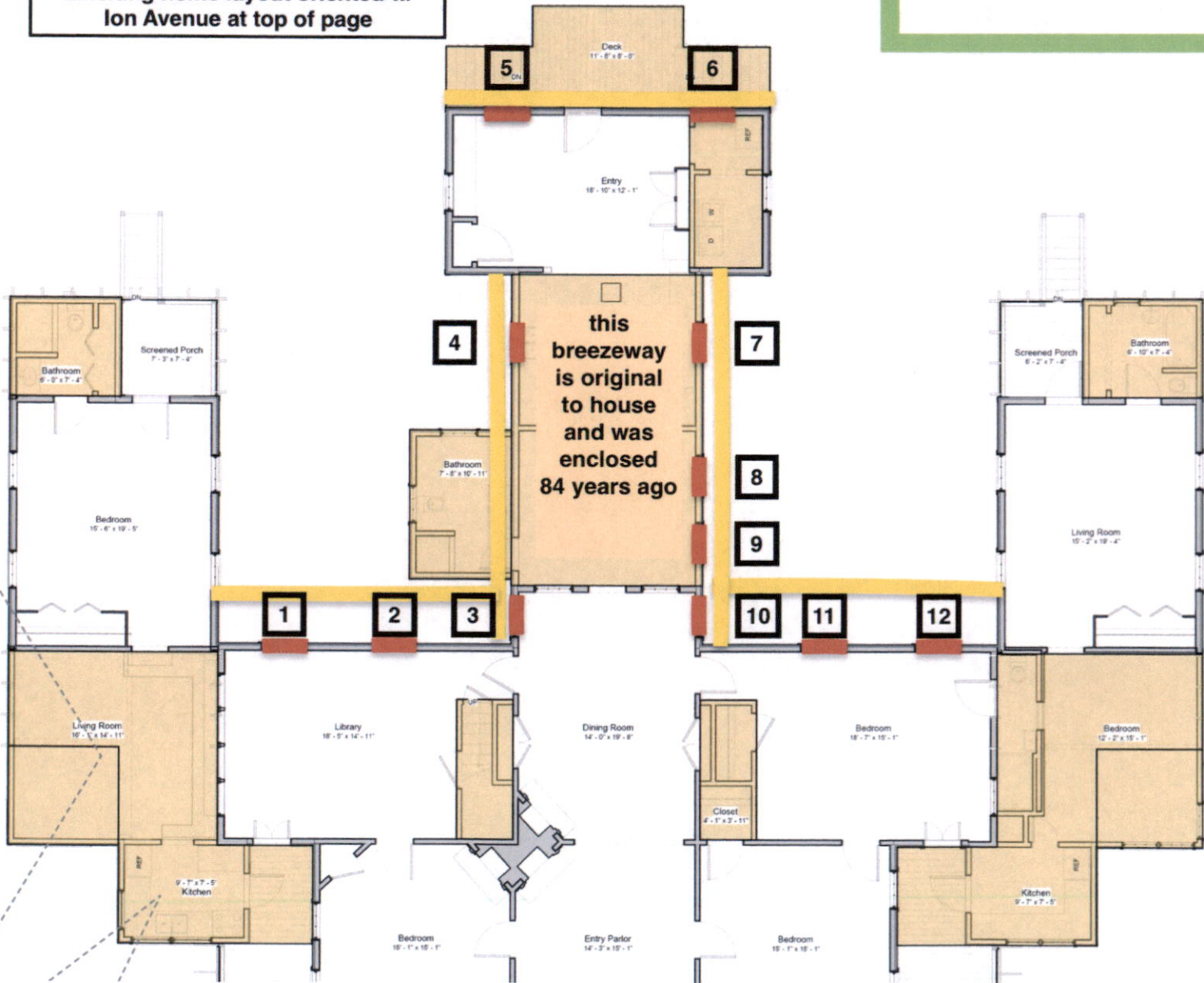
5 = 5 historic Facades that will be partially or totally gutted.

Existing home layout oriented w/
Ion Avenue at top of page

How Big are the Cuts?



the summer house
dates back to at
least the 1970's



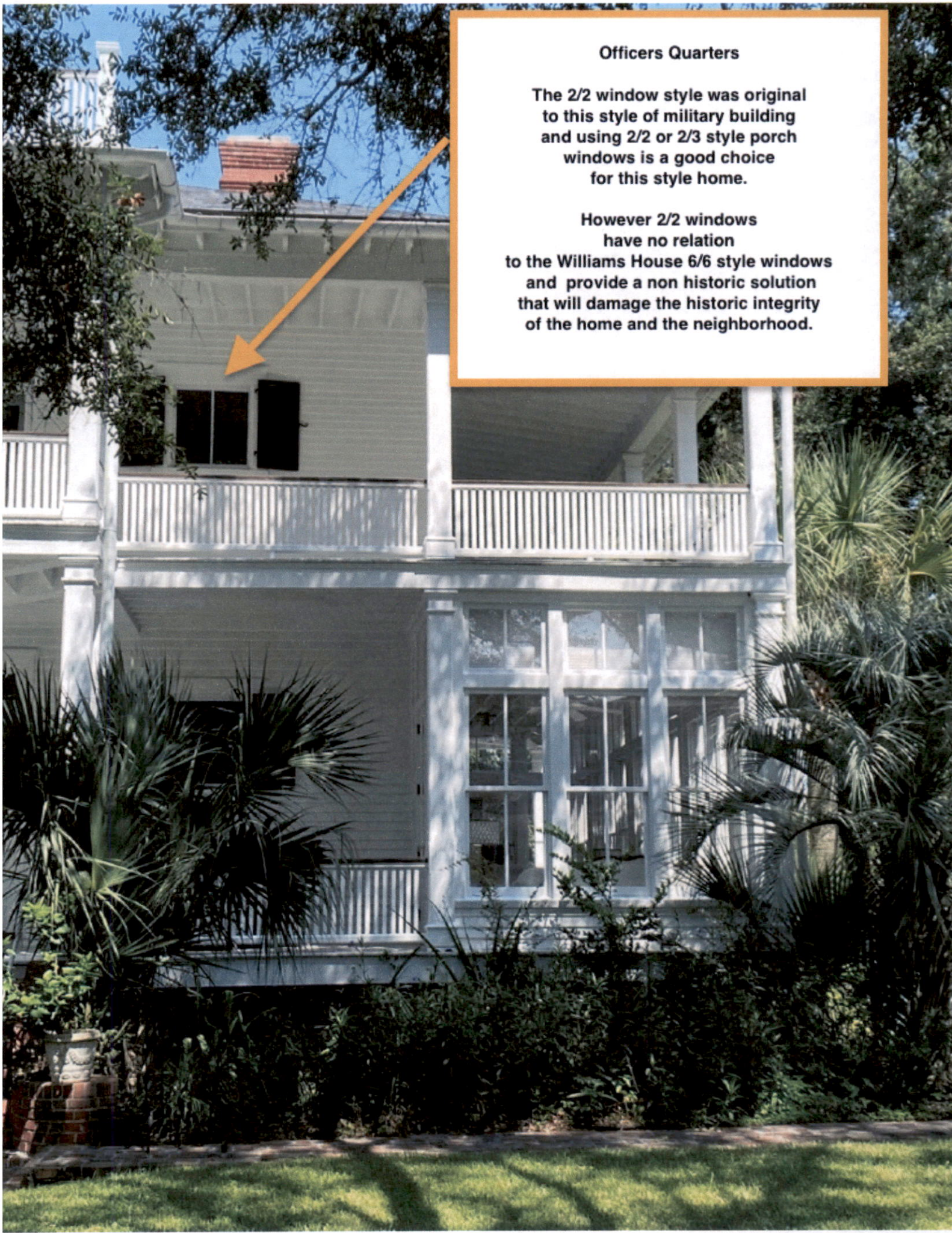
These changes are significant and adverse. They erase the connection to the past for the house and neighbors. The new materials do not match or reflect the historic materials, style or workmanship, degrading the integrity of this 124 year old treasure. The historic character of this community will be diminished. It will create a precedent against historic preservation.

GUIDELINES FOR REHABILITATION
1. The contractor shall provide a written narrative description and restoration sequencing. AT NO COST DURING THE RECONSTRUCTION SHALL LABOR OF THE EXTERIOR ENVELOPE OF THE HISTORIC

Officers Quarters

The 2/2 window style was original to this style of military building and using 2/2 or 2/3 style porch windows is a good choice for this style home.

However 2/2 windows have no relation to the Williams House 6/6 style windows and provide a non historic solution that will damage the historic integrity of the home and the neighborhood.



**School House/Condo Renovation
at 2302 Middle Street**

**This renovation is a respectful and historic solution for
the Williams house
using 6/6 historic windows**





**Interior view of School House/Condo 6/6
2302 Middle Street**

**Using the same 6/6 window style
throughout the Williams house maintains
the historic integrity and provides visual
continuity, while solving for open, bright
living space.**





From the National Register of Historic Places Sullivan's Island

page 13-14 revised 2002

"A resource should retain its overall historic form, its architectural detailing and continue to possess materials that evoke the period of construction or historic alteration."

Integrity Standards

Materials: Resort houses on Sullivan's Island will be considered to retain integrity of materials where they retain a substantial percentage of original materials or materials that are over 50 years old, or where modern replacement materials match documented historic conditions in material, design, and workmanship.

Feeling: Integrity of feeling gives a property is sense of time and place. Each historic resource on Sullivan's Island should evoke its connection with the past, and its place in the overall history of the area.

Association: A property should have integrity of association, the relationship between the place and its chain of owners and its community.



RECUSAL STATEMENT

Member Name: Phil Clarke

Meeting Date: Feb. 21, 2024

Agenda Item: 4 Section: F Number: 4

Topic: 2314 Brooks

The Ethics Act, SC Code §8-13-700, provides that no public official may knowingly use his office to obtain an economic interest for himself a family member of his immediate family, an individual with whom he is associated, or a business with which he is associated. No public official may make, participate in making, or influence a governmental decision in which he or any such person or business has an economic interest. Failure to recuse oneself from an issue in which there is or may be conflict of interest is the sole responsibility of the council member (1991 Op. Atty. Gen. No. 91-37.) A written statement describing the matter requiring action and the nature of the potential conflict of interest is required.

Justification to Recuse:

Professionally employed by or under contract with principal

Owns or has vested interest in principal or property

Other: _____

Member Signature

_____ Date

Signature of Official

2/21/24
Date

Re: "The Sandlot"
2620 Atlantic Avenue

Dear Sullivan's Island Neighbors,

My Parents built their retirement age home at 2620 Atlantic Avenue over 20 years ago. After my mother's passing in 2015, my father resided there until his passing this April 2023.

As the owner of 2620 Atlantic, I will ultimately be selling my home in Mt. Pleasant and becoming a full-time resident on the island. I have partnered professionally with Ross Ritchie, Loyal Architects, to renovate the home before I relocate there full-time.

My hope is to honor my parents' original intent of the creation of a traditional style family home. While I will update the interior, I will not be altering the original footprint of the house itself.

On the exterior, I would like to add a pool, outdoor deck, and gazebo area on the Atlantic Avenue side. The original interior and exterior house design creates a natural flow from front door entry on the Ion side to the screened porch on the rear side of the residence. Our design and placement of a pool on the Atlantic Avenue side allows us to maintain the essence of the original home. Aesthetically, the visual from Atlantic Avenue still preserves 57 feet of open grass/lawn area from the pool/outdoor deck to the street.

The attached renderings show the house as it will appear if completed with a plan for an above ground pool, outdoor deck and gazebo on the Atlantic Avenue side. There will ultimately be less concrete paving on both the Ion and Atlantic sides.

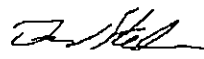
We will be seeking approval of the plans, primarily pool placement, at the November meeting of the Design Review Board. My hope is to communicate the overall house plan concept to neighbors and have them sign the attached document in support of our design.

Respectfully,

Catherine L. Stuhr
843-200-1976
Catherine.stuhr@jhenrystuhr.com

With my signature, I acknowledge my support of the proposed renovations as outlined above to 2620 Atlantic Avenue.

Name (printed): David Stedman

Signature: 

Address: 2620 Bayonne St.

Phone #: _____

Exhibit 4

Charles,

I encourage the TOSI to support the construction of the parking lot at 2118 Ion Ave with as few restrictions as possible. I believe this is a golden opportunity to improve conditions around the Commercial District. Off street parking availability entirely within the Commercial District can eliminate a lot of problems in residential areas. One example is the Battery Thomson parking lot. Battery Thomson is on the National Historic Register. The lot never went through the approval process and probably wouldn't have been approved if it did. It's also located in a residential area and drivers often use Ion Avenue as a Commercial District bypass to get to and from it. Finally a Commercial District parking lot does not belong in a residential area next to a school. There are many other parking issues around the Commercial District.

Beach traffic parking and congestion will always be part of living on this island but that doesn't mean residents should have to deal with problems after beach traffic dies down. Maximizing off street parking within the Commercial District is the answer, not a problem. With plenty of available off street parking within the Commercial District many problem areas and concerns can be dealt with a lot easier. I believe with proper planning and honest intent, the residents around the Commercial District will be better off with the approval of 2118 parking. Thank You, Rick Graham. 2102 Ion Ave.

BENEFITS— — More parking within the Station 21 Commercial District can mean less parking and traffic in residential areas and is an opportunity that shouldn't be missed. The fewest artificial restrictions on 2118 parking will benefit the most residents instead of a handful.

ROAD BLOCKAGE— — Cars jutting into the Station 22 roadway and the excessive number of trash cans blocking the right of way has long been a problem. The town should fix the problems regardless of 2118 Ion.

BEACH PATH— — There are 2 other side streets between the Fire Station and Station 22 which are used when Middle Street is congested. Popular beach times and Middle Street congestion go together. Our Police and Firemen aren't going to sit in line waiting to turn on Station 22.

PEDESTRIANS AND TRAFFIC— — 2118 parking provides pedestrians access in either direction via sidewalks and crosswalks to all of the Commercial District. The illegal parking at Batteries Thomson and Gadsden, often with over 75 cars parked between them, is the source of most of the pedestrian and car traffic at intersections on Ion and elsewhere some are concerned with.

ACCESSIBILITY— — The parking lot needs to be accessible and we shouldn't trap drivers in or out of it which will only add to congestion when traffic backs up. If this lot is busy during the day it means cars will be everywhere anyway and it really is part of living here. The Station 22 curb cut is the best place for it. Drivers need to get in and out during the day but can easily be directed towards Middle Street in the evening hours which will keep them out of residential neighborhoods.

Rick Graham
2102 ION

Exhibit 8

To: Zoning/Planning/DRB/Town Council

January 29.24

It has come to my attention that a parking lot is proposed on the corner of Middle Street and Station 22.

I am surprised that the current investor bought this lot with the intent to use it as a parking lot when the other new lot on Middle Street is empty almost all of the time.

Of even larger concern is the entrance on Station 22, the most direct and active emergency route on Sullivan's Island. Blocking this road and making it more difficult for emergency vehicles to enter 22 and access the beach is unconscionable.

Additionally, this property borders a neighborhood on two sides. The residents already contend with more than a dozen garbage bins, dumpsters and trucks parked on 22 making the left turn onto 22 often problematic. Golf cart parking and kids on bikes and foot are abundant in this area as well as it is an active cross walk and direct route to the park. Additionally, it is also used as a loop to get to IOP now that a left on Middle Street getting on the island is prohibited.

Please reconsider this and the real and long term intention and impact of this move. This change not only diminishes swift emergency access to the beach, it represents an extreme safety hazard to residents and opens a gateway for potential commercial sprawl on Sullivan's Island.

Kind Regards,

Kimberly Brown
2118 Pettigrew Street

Jessi Gress

Exhibit 9

From: Charlotte Artus <Charlotte@artusart.com>
Sent: Sunday, February 4, 2024 9:48 AM
To: Jessi Gress; Bridget Welch
Subject: Zoning on Station 22.

CAUTION: > This email originated from outside the Town of Sullivans Island. Do not click links or open attachments unless you have verified the sender and know the content is safe.

Hello,

I am writing with respect to your zoning meeting on February 8, 2024, to consider allowing a new parking lot entrance on Station 22 between Middle Street and Ion.

That block is already extremely congested, teeming with pedestrians, cars and trucks, deliveries, golf carts, and bicycles. Adding a point of ingress and egress directly across from another parking area holding 5 or so cars and large trucks, and golf cart parking, as well as turning traffic from Middle Street (often too fast and cutting the corner) would considerably increase the risk of vehicle collisions.

The intersections of Station 22 with Ion and Atlantic Avenue are quite perilous, as many visitors think those are 4 way stops... near misses are daily. (Please can we also consider putting up signs at those intersections to say "Cross Traffic does not Stop" as we have at other intersections close by?).

Even more dangerous though, would be the risk to pedestrians. Station 22 is the most visited entrance for beach goers (especially recently as many beach paths are closed by high water). Our visitors to the downtown restaurants, bars, and shops, our neighbours out for a stroll with children and dogs, have no where else to walk but in the street on Station 22. Pedestrians are walking on that street day and night. Adding another hazard on that block would be unconscionable.

Station 22 is also the point of entry for emergency services. We should not even contemplate making the jobs of the emergency responders more difficult by adding to the congestion on Station 22.

Middle Street is already designed to allow for ingress and egress for parking.

Middle Street has sidewalks for pedestrians, and is wide enough to allow for two vehicles to easily pass, or for a vehicle to go around deliveries, for instance. Please do not bring this commercial activity into our narrow, busy and residential side street at Station 22.

All of us who live near Station 22 on Ion, Atlantic, and Pettigrew Streets, have the right to safety in our neighbourhood, and the duty to ensure that care for our visitors when we can.

Please do not approve this zoning request.

Yours truly,
Charlotte and Alan Artus
2114 Pettigrew Street
Sullivan's Island, SC

Comments regarding proposed parking lot at Sta. 22

I live at the corner of Sta. 22 and Atlantic. My family and I drive between Atlantic and Middle St. at least 6 times a day. The portion between Ion and Middle is easily our most complained about section of any S.I. street. Even with parking on only one side, it is difficult to have 2 cars pass each other because of the width of the drivable area of the street. This is greatly complicated by the large numbers of people walking on the street, frequently 4 or more abreast. Over the past 20 years, both the foot and vehicular traffic have increased dramatically, and winter traffic now is what summer traffic was back then. After the installation of the right turn only lane onto Middle at the gas station, there was an immediate great increase in Sta. 22 traffic caused by tourists crossing from Mt. Pleasant. The tourists often do not appreciate the prior legal left onto Jasper, so they turn on Middle and make the first left down Sta. 22 attempting to go back toward IOP. All this has made it hard to transverse that first block of Sta. 22; a parking lot entrance would make this section of road turn from difficult to almost impossible. Parkers making a left turn onto Sta. 22 would back up traffic on Middle quite a bit more than present, which already is far from good. I can see the danger of people crossing between Poe's and the other side by weaving around stopped cars growing much worse. I think an entrance to the proposed lot from Middle makes much more sense. It is a little further from the heavy commercial area, and already less congested. There does not appear to be much back-up caused by the present parking lot which was built there a year or 2 ago, so a new lot entrance should be fine. If traffic on Middle started to back up due to cars turning into the 2 lots, I propose eliminating parking on the north side of Middle for 100 feet or so from Sta. 22, and dedicating a left turn only lane for the new lot, and the old lot as well. This may not be necessary, because the vast majority of parking would be after 5:30 PM when the traffic going east is very light and allows for easy left turns for west-bound Middle St. traffic.

I would be happy to discuss this further.

Mark Reinhardt

2201 Atlantic Ave.

Cell # 843 364-1948

Exhibit 11

SITE DEVELOPMENT PLANS FOR

MIDDLE STREET PARKING

MIDDLE STREET SULLIVAN'S ISLAND, SOUTH CAROLINA

SITE CONTACTS

D4 PARTNERS DANE DERBYSHIRE 450 MEETING ST CHARLESTON, SC 29403 PHONE: (843) 958-0540	CIVIL ENGINEER MATTHEW CLINE, P.E. CLINE ENGINEERING P.O. BOX 31204 CHARLESTON, SC 29413 PHONE (843) 991-7239	PROJECT SURVEYOR E.M. SEABROOK 1017 CHUCK DAWLEY BLVD, SUITE 200 PO BOX 96 MOUNT PLEASANT, SC 29465 PHONE (843) 844-4496
SOUTH CAROLINA EROSION CONTROL S.C.D.H.E.C. O.G.R.M. RICHARD GIER STORMWATER DIVISION 1162 MCMILLAN AVE, SUITE 400 CHARLESTON, SC 29405 PHONE (843) 953-0200	SULLIVAN'S ISLAND PLANNING AND ZONING JOE HENDERSON, AICP 2056 MIDDLE STREET SULLIVAN'S ISLAND, SC 29443 PHONE (413) 843-3731	

ADA COMPLIANCE

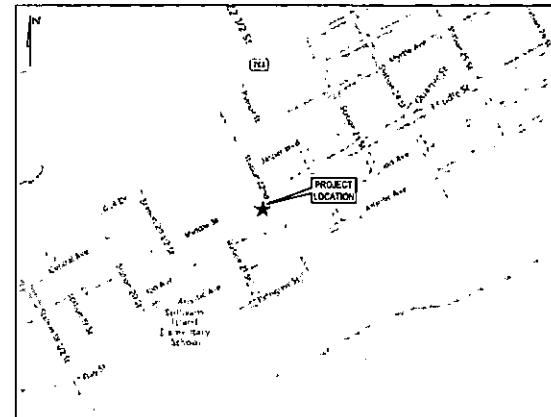
THE PUBLIC RIGHT-OF-WAY MUST REMAIN ADA COMPLIANT DURING CONSTRUCTION OR AN ALTERNATIVE ROUTE MUST BE PROVIDED. IT IS THE OWNERS RESPONSIBILITY TO REPAIR ALL DAMAGED SIDEWALKS TO REINSTATE AN ADA ACCESSIBLE ROUTE.

PROJECT DESCRIPTION

SITE IMPROVEMENTS WILL CONSIST OF THE INSTALLATION OF GRAVEL PARKING, DRIVEWAY AND LANDSCAPING

SHEET LIST TABLE

SHEET NUMBER	SHEET TITLE
G-1	COVER
G-2	GENERAL NOTES
G-3	GENERAL NOTES
V-1	SITE CONDITIONS
C-1	SITE PLAN
C-2	DEMOLITION, TREE PROTECTION AND SWPPP
C-3	GRADING AND DRAINAGE PLAN
C-4	HYDROLOGY PLAN
C-5	CONSTRUCTION DETAILS
C-6	CONSTRUCTION DETAILS
L-1	SITE PLAN



VICINITY MAP

SCALE 1" = 500'

REV.	DATE	DESCRIPTION	DESIGNER	DATE	DESCRIPTION	DATE	APPROVED

CLINE
ENGINEERING
PROFESSIONAL DESIGN CONSULTING

COVER
INTERSECTION OF MIDDLE STREET AND STATION 22 STREET
SULLIVAN'S ISLAND, SOUTH CAROLINA

SHEET NUMBER
G-1

Table with columns: NO., DATE, REVISION, DESCRIPTION, DRAWN BY, CHECKED BY, APPROVED BY.

- Legend symbols and their corresponding descriptions: 1. 2. 3. 4. 5. 6. 7. 8. 9. 10. 11. 12. 13. 14. 15. 16. 17. 18. 19. 20. 21. 22. 23. 24. 25. 26. 27. 28. 29. 30. 31. 32. 33. 34. 35. 36. 37. 38. 39. 40. 41. 42. 43. 44. 45. 46. 47. 48. 49. 50. 51. 52. 53. 54. 55. 56. 57. 58. 59. 60. 61. 62. 63. 64. 65. 66. 67. 68. 69. 70. 71. 72. 73. 74. 75. 76. 77. 78. 79. 80. 81. 82. 83. 84. 85. 86. 87. 88. 89. 90. 91. 92. 93. 94. 95. 96. 97. 98. 99. 100.

- Legend symbols and their corresponding descriptions: 1. 2. 3. 4. 5. 6. 7. 8. 9. 10. 11. 12. 13. 14. 15. 16. 17. 18. 19. 20. 21. 22. 23. 24. 25. 26. 27. 28. 29. 30. 31. 32. 33. 34. 35. 36. 37. 38. 39. 40. 41. 42. 43. 44. 45. 46. 47. 48. 49. 50. 51. 52. 53. 54. 55. 56. 57. 58. 59. 60. 61. 62. 63. 64. 65. 66. 67. 68. 69. 70. 71. 72. 73. 74. 75. 76. 77. 78. 79. 80. 81. 82. 83. 84. 85. 86. 87. 88. 89. 90. 91. 92. 93. 94. 95. 96. 97. 98. 99. 100.

ABBREVIATIONS table listing various symbols and their meanings: 1. 2. 3. 4. 5. 6. 7. 8. 9. 10. 11. 12. 13. 14. 15. 16. 17. 18. 19. 20. 21. 22. 23. 24. 25. 26. 27. 28. 29. 30. 31. 32. 33. 34. 35. 36. 37. 38. 39. 40. 41. 42. 43. 44. 45. 46. 47. 48. 49. 50. 51. 52. 53. 54. 55. 56. 57. 58. 59. 60. 61. 62. 63. 64. 65. 66. 67. 68. 69. 70. 71. 72. 73. 74. 75. 76. 77. 78. 79. 80. 81. 82. 83. 84. 85. 86. 87. 88. 89. 90. 91. 92. 93. 94. 95. 96. 97. 98. 99. 100.

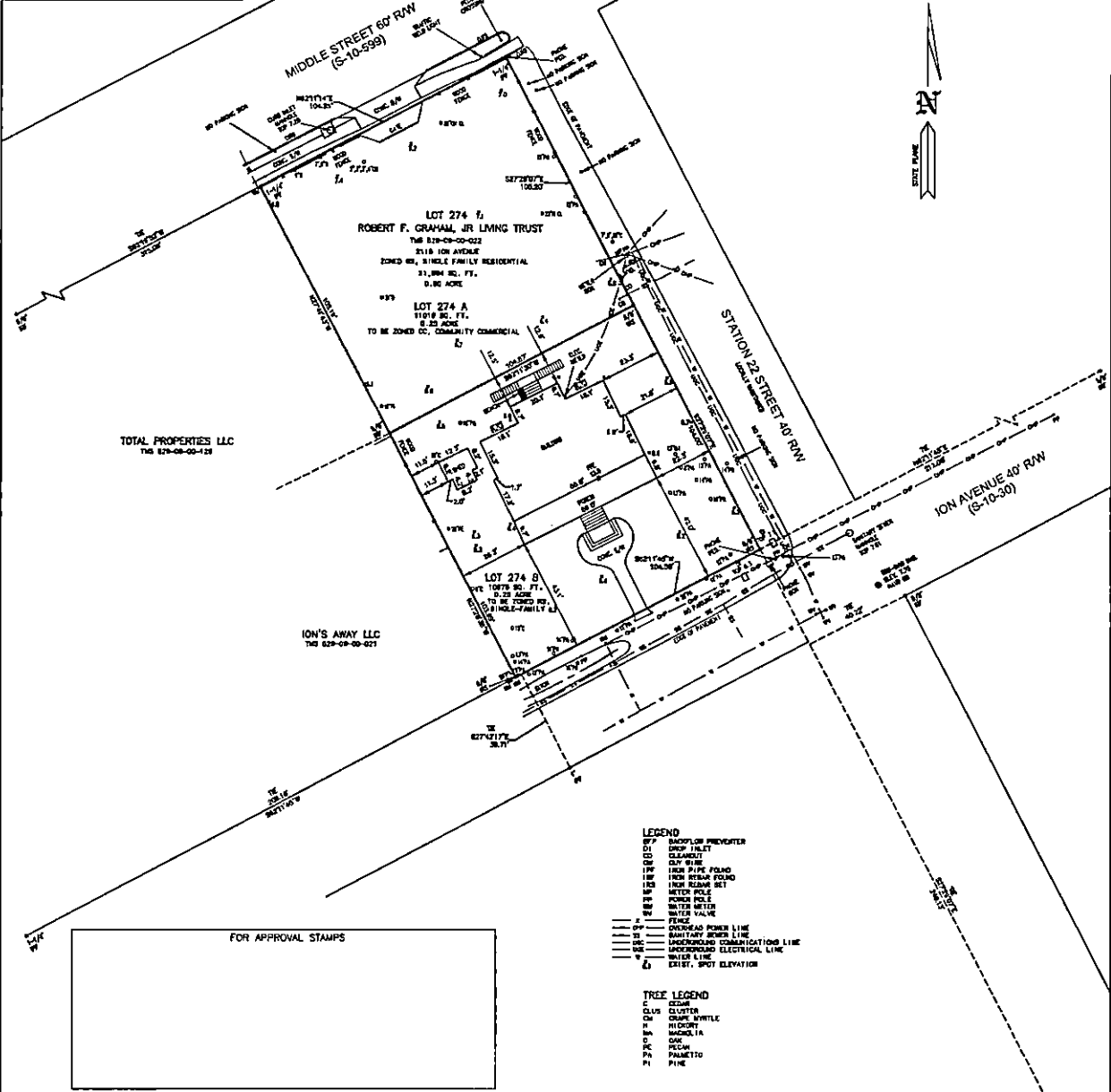
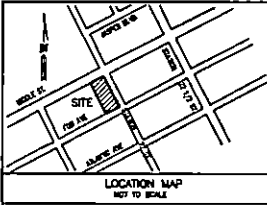
WATER LINES
1. ALL WATER LINES SHALL BE INSTALLED IN ACCORDANCE WITH THE PROVISIONS OF THE MISSOURI PLUMBING CODE AND THE CITY OF SULLYVILLE PLUMBING ORDINANCES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE LOCAL HEALTH DEPARTMENT AND THE CITY ENGINEER. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL EXISTING WATER LINES AND SHALL BE RESPONSIBLE FOR THE REPAIR AND REPLACEMENT OF ANY DAMAGED EXISTING WATER LINES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL EXISTING WATER LINES AND SHALL BE RESPONSIBLE FOR THE REPAIR AND REPLACEMENT OF ANY DAMAGED EXISTING WATER LINES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL EXISTING WATER LINES AND SHALL BE RESPONSIBLE FOR THE REPAIR AND REPLACEMENT OF ANY DAMAGED EXISTING WATER LINES.

SCHEMATIC
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MISCELLANEOUS
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SANITARY SEWER LINES
1. ALL SANITARY SEWER LINES SHALL BE INSTALLED IN ACCORDANCE WITH THE PROVISIONS OF THE MISSOURI PLUMBING CODE AND THE CITY OF SULLYVILLE PLUMBING ORDINANCES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE LOCAL HEALTH DEPARTMENT AND THE CITY ENGINEER. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL EXISTING SANITARY SEWER LINES AND SHALL BE RESPONSIBLE FOR THE REPAIR AND REPLACEMENT OF ANY DAMAGED EXISTING SANITARY SEWER LINES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL EXISTING SANITARY SEWER LINES AND SHALL BE RESPONSIBLE FOR THE REPAIR AND REPLACEMENT OF ANY DAMAGED EXISTING SANITARY SEWER LINES.

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TOTAL PROPERTIES LLC
TNS 829-09-00-128

ION'S AWAY LLC
TNS 829-09-00-027

FOR APPROVAL STAMPS

- LEGEND**
- DTA BROADBAND PROVIDER
 - D1 DROP INLET
 - CO CLEARCUT
 - CLW CLAY WIRE
 - IFP IRON PIPE FOUND
 - IFR IRON REBAR FOUND
 - IES IRON REBAR SET
 - MP METER POLE
 - SWP WATER METER
 - WV WATER VALVE
 - F FENCE
 - PPV OVERHEAD POWER LINE
 - ST SANITARY SEWER LINE
 - UC UNDERGROUND COMMUNICATIONS LINE
 - UE UNDERGROUND ELECTRICAL LINE
 - WL WATER LINE
 - E EXIST. SPOT ELEVATION
- TREE LEGEND**
- C CEDAR
 - CLUS CLUSTER
 - CM CHAMP WHITEC
 - H HICKORY
 - M MAGNOLIA
 - O OAK
 - PC PECAN
 - PA PALMETTO
 - PI PINE

ZONING:
 R1, SINGLE-FAMILY DETACHED ARE:
 FRONT - 25 FT.
 SIDES - 15 AND 20 FT., OR 30 AND 20 FT., OR ANY COMBINATION BETWEEN THE 2 THAT EQUALS 40 FT., COMBINED
 REAR - 25 FT.
 C2, COMMUNITY COMMERCIAL DETACHED ARE:
 FRONT - 8 FT., MIN AND 18 FT., MAX
 SIDES - 5 FT., MIN (ONLY WHEN ATTACHED TO ADJACENT BUILDING) TO 8 FT., MAX
 REAR - 15 FT., MIN.

REFERENCES:
 1. PLAT OF THE WESTERN END OF SULLIVANS ISLAND DATED MAY 1 1964
 RECORDED IN CHARLESTON COUNTY RECORD OFFICE
 IN PLAT BOOK 8, PAGE 83
 2. PLAT BY THOMAS & HUTTON ENGINEERING CO. DATED MAR. 8, 2012
 RECORDED IN CHARLESTON COUNTY RECORD OFFICE
 IN PLAT BOOK 114, PAGE 0274.

**MOULTRIEVILLE
 TOWN OF SULLIVANS ISLAND
 CHARLESTON COUNTY, S. C.**
 PLAT OF THE SUBDIVISION OF LOT 274 (TNS 520-09-00-022)
 CONTAINING 0.50 ACRE INTO
 LOT 274 A CONTAINING 0.25 ACRE AND LOT 274 B CONTAINING 0.25 ACRE
 OWNED BY ROBERT F. GRAHAM, JR LIVING TRUST
 ABOUT TO BE CONVEYED TO D4 PARTNERS, LLC

SCALE: 1" = 20'
 20 10 0 20 40 60
 SCALE IN FEET
 DECEMBER 7, 2023

NOTES:
 1. AREA DETERMINED BY COORDINATES
 BASED ON INTERPRETATION OF FIRM FLOOD
 INSURANCE RATE MAP NUMBER 45013C 0203 K
 DATED JANUARY 25, 2021. THE PROPERTY
 NUMBER MOUND LIES IN FLOOD ZONE
 AE (FEET - 10').
 2. THERE IS NO OBSERVABLE EVIDENCE OF THE LOCATION OF
 CENTERLINE OF SEWER MAINS ON THIS PROPERTY.
 SURFACE CONDITIONS WERE NOT EXAMINED OR CONSIDERED
 AS PART OF THIS SURVEY. NO STATEMENT IS MADE CONCERNING
 ANY SURFACE CONDITIONS THAT MAY AFFECT THE USE
 OR FURTHER DEVELOPMENT OF THIS PROPERTY.

LEWIS E. SEABROOK
 CIVIL ENGINEER & LAND SURVEYOR
 S. C. REG. NO. 38860
 P. O. BOX 86
 MOULTRIEVILLE, S. C. 29908
 (843) 884-4408



NAVD 88

SITE CONDITIONS

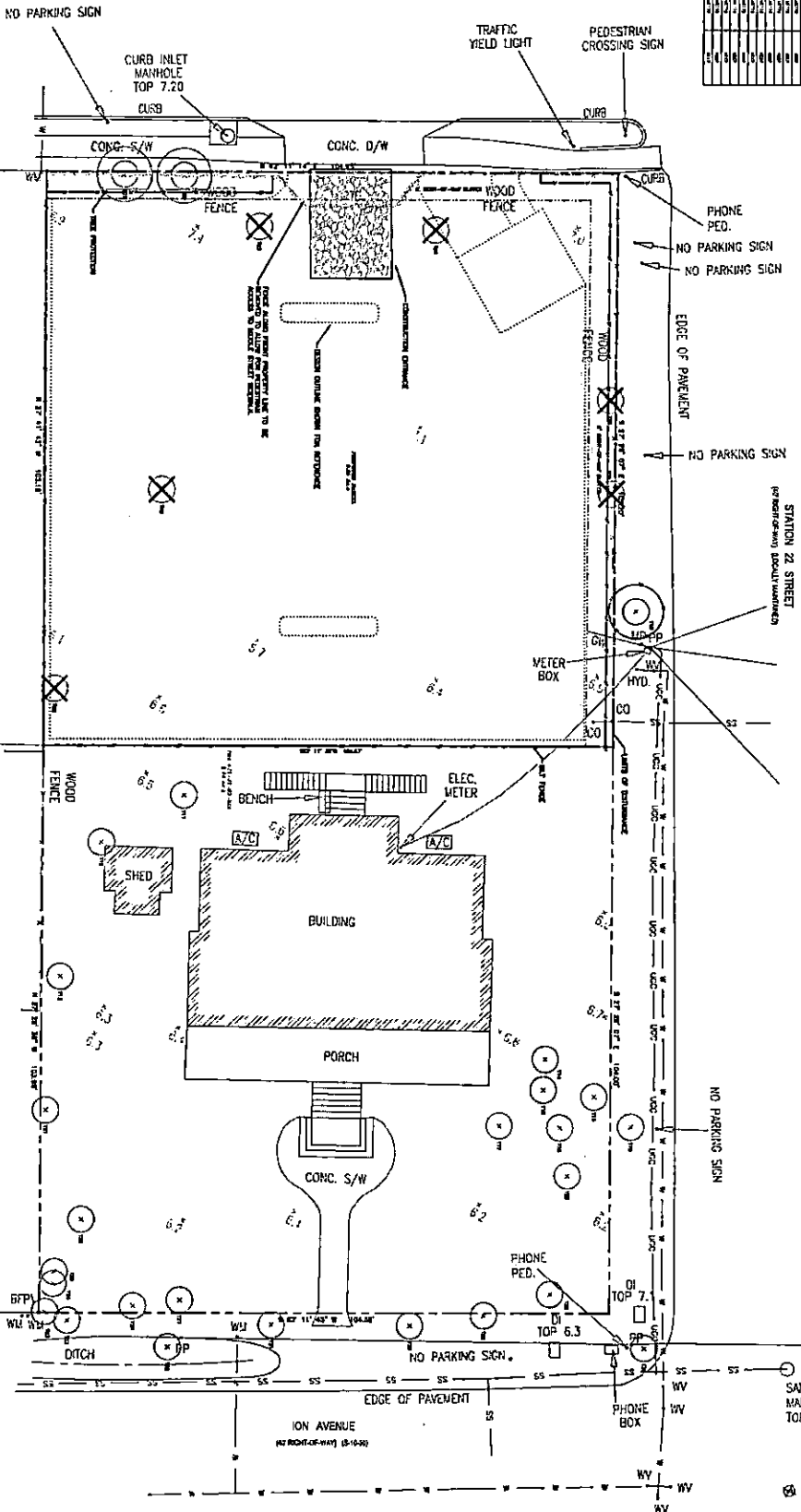


SCB
 South Carolina
 Professional Design Consulting
 1000 Highway 17, Suite 100
 Columbia, SC 29204
 (803) 792-8888
 www.scbdesign.com

MIDDLE STREET
 (A 60' x 21' - 0" x 21' - 0")

NO.	DATE	DESCRIPTION	BY	CHKD.
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TREE MITIGATION CALCULATION	
TOTAL TREE VOLUME TO BE REMOVED (TV)	100.00
TOTAL TREE VOLUME TO BE PRESERVED (TP)	100.00
TOTAL TREE VOLUME TO BE REPLANTED (TR)	100.00
TOTAL TREE VOLUME TO BE MAINTAINED (TM)	100.00
TOTAL TREE VOLUME TO BE DEMOLISHED (TD)	100.00
TOTAL TREE VOLUME TO BE REPLANTED (TR)	100.00
TOTAL TREE VOLUME TO BE MAINTAINED (TM)	100.00
TOTAL TREE VOLUME TO BE DEMOLISHED (TD)	100.00

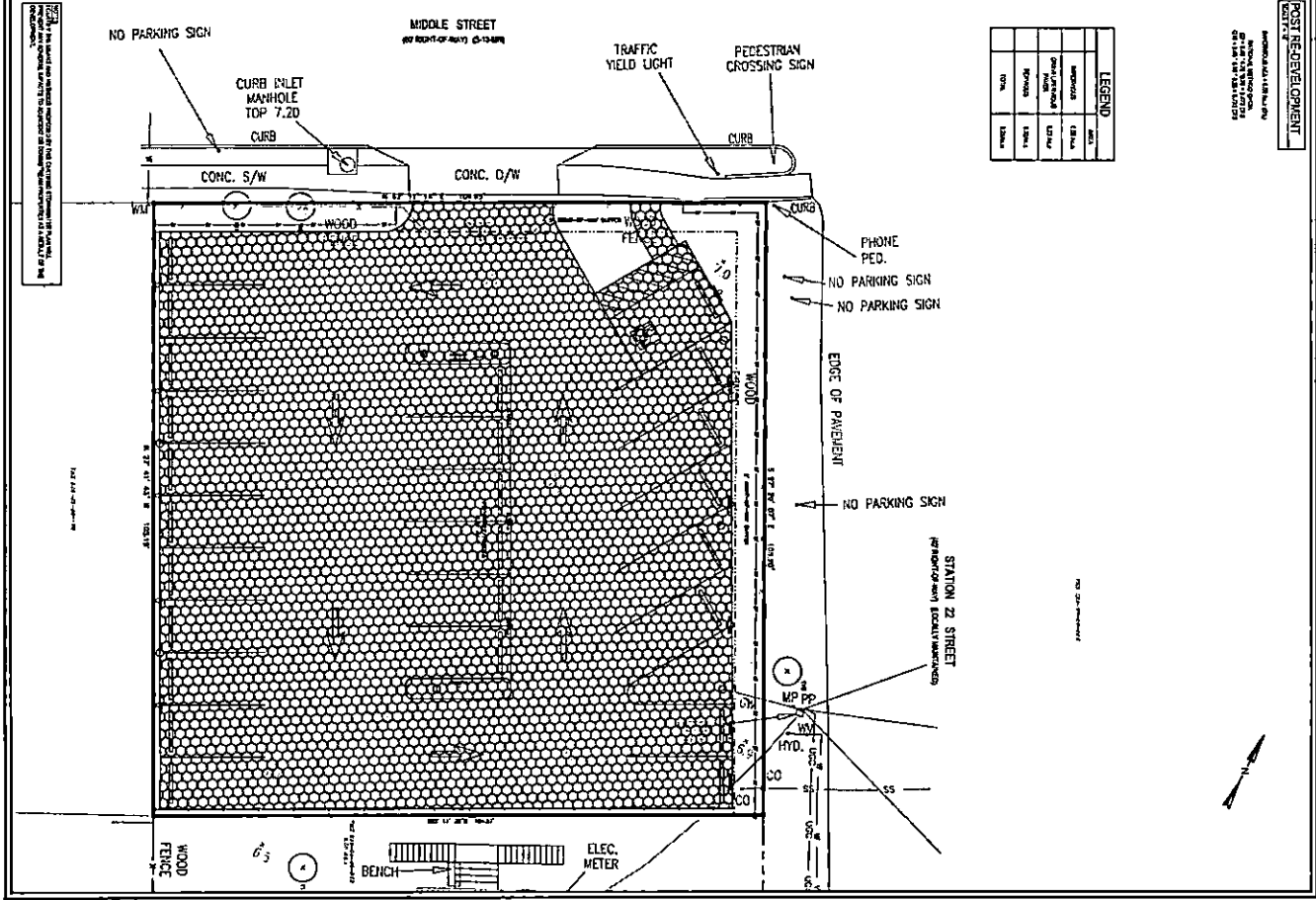
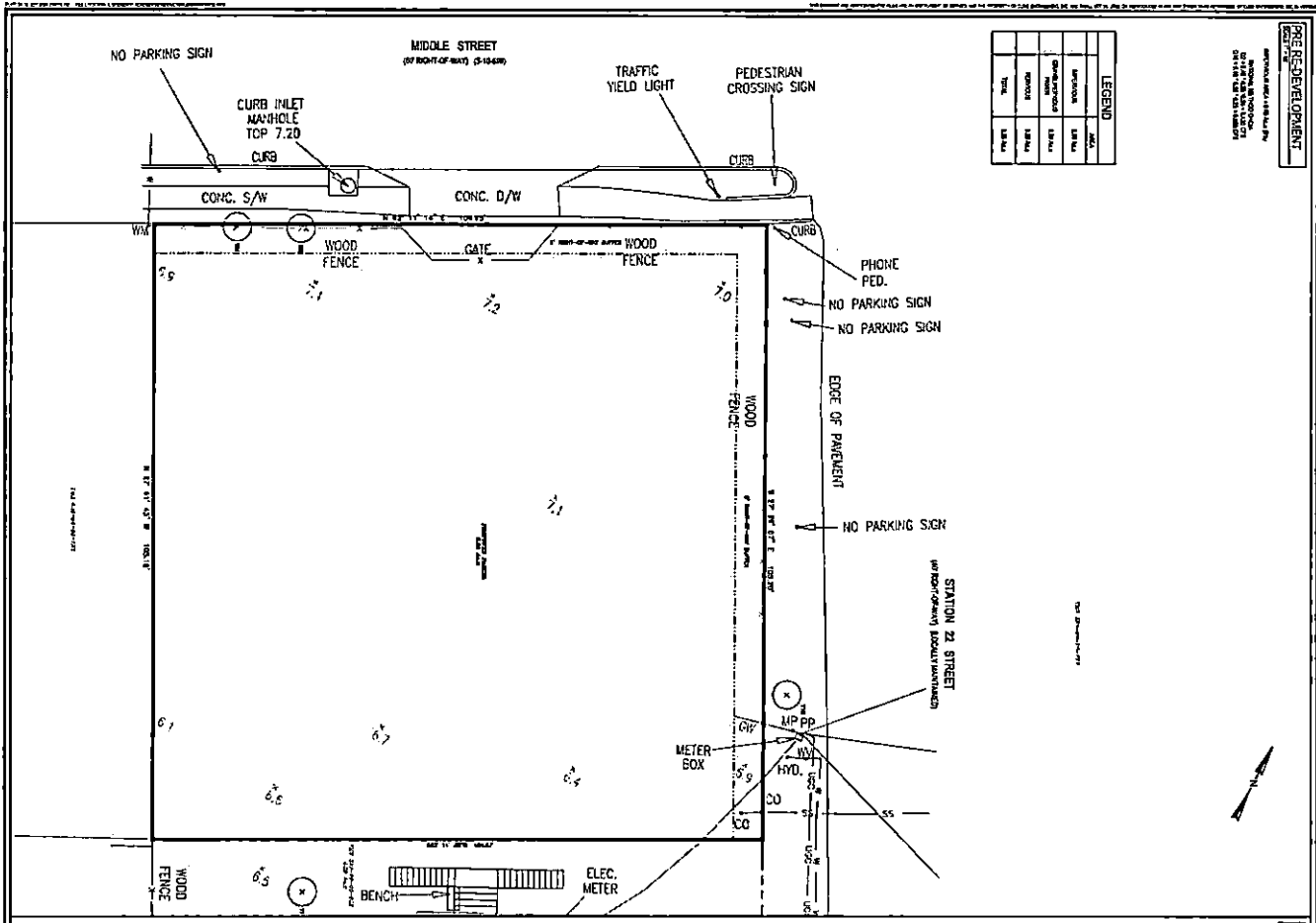


C-2

MIDDLE STREET PARKING
**DEMOLITION, TREE PROTECTION
 AND SWPPP**
 INTERSECTION OF MIDDLE STREET AND STATION 22 STREET
 SHELBY COUNTY, SOUTH CAROLINA



REV.	DATE	DESCRIPTION	DRAWN	APPR.
A	8/18/2021	ISSUED FOR REVIEW AND APPROVAL	AJ	MCC
B	8/27/2021	ISSUED FOR REVIEW AND APPROVAL	AJ	MCC
C	8/27/2021	REVISED PER SVA COMMENTS	AJ	KEC



POST-REDEVELOPMENT

DATE: 08/14/2014
 DRAWN BY: J. B. BROWN
 CHECKED BY: J. B. BROWN
 APPROVED BY: J. B. BROWN

LEGEND	
CONCRETE	1/4" = 1'-0"
WOOD FENCE	1/4" = 1'-0"
WOOD FENCE	1/4" = 1'-0"
WOOD FENCE	1/4" = 1'-0"
WOOD FENCE	1/4" = 1'-0"
WOOD FENCE	1/4" = 1'-0"
TOTAL	1/4" = 1'-0"

POST-REDEVELOPMENT

DATE: 08/14/2014
 DRAWN BY: J. B. BROWN
 CHECKED BY: J. B. BROWN
 APPROVED BY: J. B. BROWN

LEGEND	
CONCRETE	1/4" = 1'-0"
WOOD FENCE	1/4" = 1'-0"
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WOOD FENCE	1/4" = 1'-0"
WOOD FENCE	1/4" = 1'-0"
WOOD FENCE	1/4" = 1'-0"
TOTAL	1/4" = 1'-0"

MIDDLE STREET PARKING

C-4

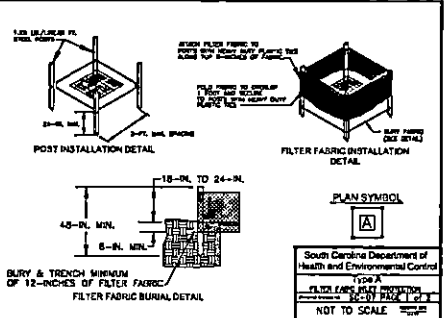
HYDROLOGY PLAN

INTERSECTION OF MIDDLE STREET AND STATION 22 STREET
 SULLYVA'S ISLAND, SOUTH CAROLINA

CLINE
ENGINEERING

PROFESSIONAL DESIGN CONSULTING

REV.	DATE	DESCRIPTION	DRAWN	APPV.
A	01/19/2014	ISSUED FOR REVIEW AND APPROVAL	AJ	MDC
B	02/17/2014	ISSUED FOR REVIEW AND APPROVAL	AJ	MDC
C	02/24/2014	REVISED PER SZA COMMENTS	AJ	MDC



South Carolina Department of Health and Environmental Control
TYPE A
 FILTER FABRIC INSTALLATION
 GENERAL NOTES
 NOT TO SCALE

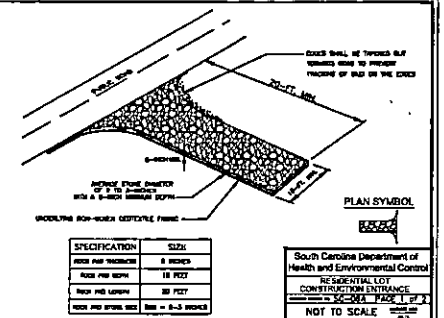
TYPE A - FILTER FABRIC REQUIREMENTS

1. The fabric shall be made of woven polypropylene fibers which are resistant to ultraviolet radiation, acids, alkalis, and other environmental conditions.
2. The fabric shall be made of woven polypropylene fibers which are resistant to ultraviolet radiation, acids, alkalis, and other environmental conditions.
3. The fabric shall be made of woven polypropylene fibers which are resistant to ultraviolet radiation, acids, alkalis, and other environmental conditions.
4. The fabric shall be made of woven polypropylene fibers which are resistant to ultraviolet radiation, acids, alkalis, and other environmental conditions.
5. The fabric shall be made of woven polypropylene fibers which are resistant to ultraviolet radiation, acids, alkalis, and other environmental conditions.
6. The fabric shall be made of woven polypropylene fibers which are resistant to ultraviolet radiation, acids, alkalis, and other environmental conditions.

TYPE A - INSPECTION & MAINTENANCE

1. The fabric shall be inspected for damage, tears, and other defects before installation.
2. The fabric shall be inspected for damage, tears, and other defects before installation.
3. The fabric shall be inspected for damage, tears, and other defects before installation.
4. The fabric shall be inspected for damage, tears, and other defects before installation.
5. The fabric shall be inspected for damage, tears, and other defects before installation.
6. The fabric shall be inspected for damage, tears, and other defects before installation.

South Carolina Department of Health and Environmental Control
TYPE A
 FILTER FABRIC INSTALLATION
 GENERAL NOTES
 NOT TO SCALE



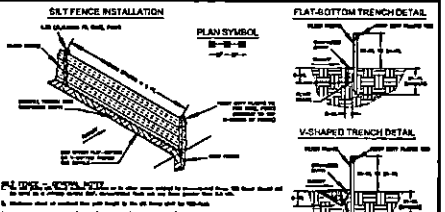
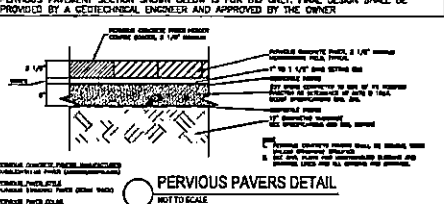
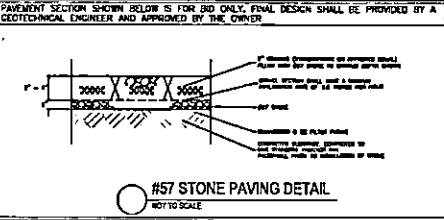
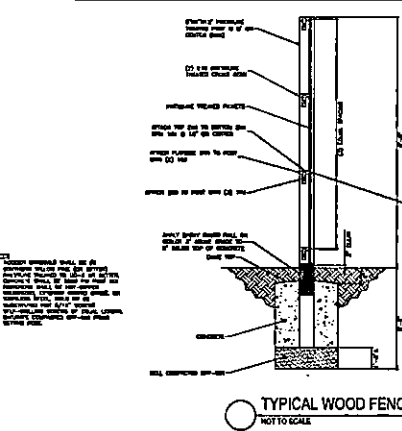
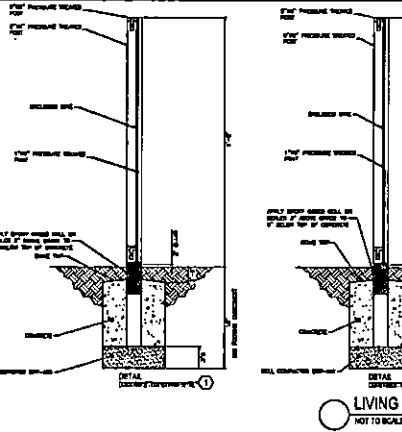
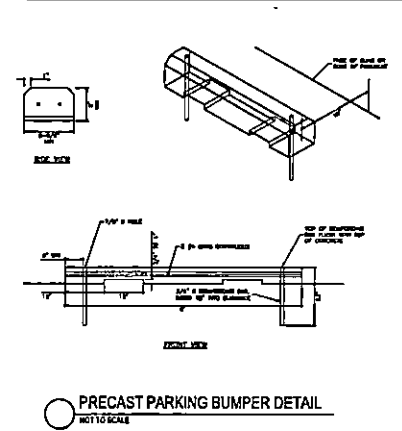
CONSTRUCTION ENTRANCE - GENERAL NOTES

1. The curb shall be constructed of concrete and shall be a minimum of 6 inches high and 18 inches wide.
2. The curb shall be constructed of concrete and shall be a minimum of 6 inches high and 18 inches wide.
3. The curb shall be constructed of concrete and shall be a minimum of 6 inches high and 18 inches wide.
4. The curb shall be constructed of concrete and shall be a minimum of 6 inches high and 18 inches wide.
5. The curb shall be constructed of concrete and shall be a minimum of 6 inches high and 18 inches wide.
6. The curb shall be constructed of concrete and shall be a minimum of 6 inches high and 18 inches wide.

CONSTR. ENTRANCE - INSPECTION & MAINTENANCE

1. The curb shall be inspected for damage, tears, and other defects before installation.
2. The curb shall be inspected for damage, tears, and other defects before installation.
3. The curb shall be inspected for damage, tears, and other defects before installation.
4. The curb shall be inspected for damage, tears, and other defects before installation.
5. The curb shall be inspected for damage, tears, and other defects before installation.
6. The curb shall be inspected for damage, tears, and other defects before installation.

South Carolina Department of Health and Environmental Control
RESIDENTIAL LOT CONSTRUCTION ENTRANCE
 GENERAL NOTES
 NOT TO SCALE



SILT FENCE - POST REQUIREMENTS

1. The posts shall be made of galvanized steel pipe and shall be a minimum of 2 inches in diameter.
2. The posts shall be made of galvanized steel pipe and shall be a minimum of 2 inches in diameter.
3. The posts shall be made of galvanized steel pipe and shall be a minimum of 2 inches in diameter.
4. The posts shall be made of galvanized steel pipe and shall be a minimum of 2 inches in diameter.
5. The posts shall be made of galvanized steel pipe and shall be a minimum of 2 inches in diameter.
6. The posts shall be made of galvanized steel pipe and shall be a minimum of 2 inches in diameter.

SILT FENCE - INSPECTION & MAINTENANCE

1. The silt fence shall be inspected for damage, tears, and other defects before installation.
2. The silt fence shall be inspected for damage, tears, and other defects before installation.
3. The silt fence shall be inspected for damage, tears, and other defects before installation.
4. The silt fence shall be inspected for damage, tears, and other defects before installation.
5. The silt fence shall be inspected for damage, tears, and other defects before installation.
6. The silt fence shall be inspected for damage, tears, and other defects before installation.

South Carolina Department of Health and Environmental Control
SILT FENCE
 GENERAL NOTES
 NOT TO SCALE



PROTECTED AND GRAND TREE PROTECTION DETAIL

South Carolina Department of Health and Environmental Control
RESIDENTIAL LOT CONSTRUCTION ENTRANCE
 GENERAL NOTES
 NOT TO SCALE

CLINE ENGINEERING
 PROFESSIONAL DESIGN CONSULTING

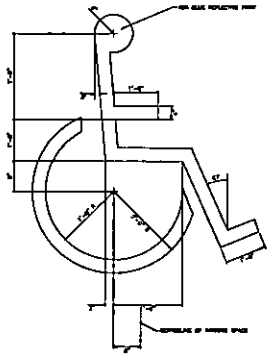
PROJECT: INTERSECTION OF MOBLE STREET AND EASTWIND PI STREET, SUMMER BEACH, SOUTH CAROLINA

DATE: 11/15/2023

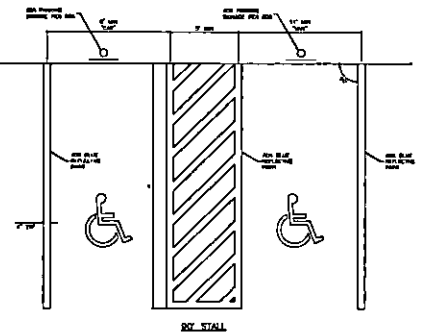
SCALE: AS SHOWN

PROJECT NUMBER: C-5

NO.	DATE	DESCRIPTION	BY	CHK
1	11/15/23	ISSUED FOR PERMIT	JK	JK
2	11/15/23	ISSUED FOR PERMIT	JK	JK
3	11/15/23	ISSUED FOR PERMIT	JK	JK
4	11/15/23	ISSUED FOR PERMIT	JK	JK



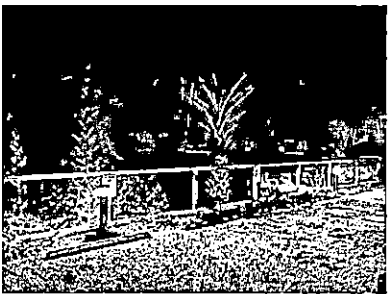
INTERNATIONAL ADA SYMBOL DETAIL
NOT TO SCALE



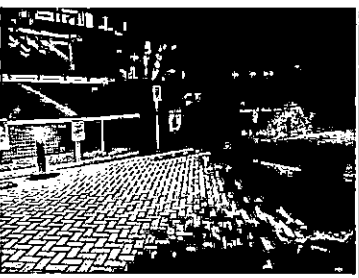
TYPICAL ADA PARKING STRIP DETAIL
NOT TO SCALE



FRONT BUFFER EXAMPLE
NOT TO SCALE



LIGHTING AND LIVING FENCE EXAMPLE
NOT TO SCALE



LIGHTING AND LIVING FENCE EXAMPLE
NOT TO SCALE

QUALITY FROM BELGARD

Belgard is a leader in the decorative paving industry. Our products are designed to enhance the appearance of your outdoor spaces while providing long-lasting performance. From residential patios to commercial plazas, Belgard has the solution for every project.

BELGARD
AQUALINE SERIES™
Commercial Collection

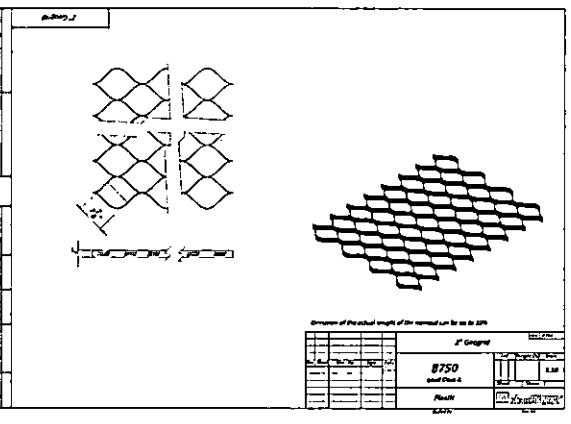
Belgard is proud to announce the launch of the Aqualine Series™ Commercial Collection. This new line of decorative paving products is designed specifically for commercial applications, offering a wide range of colors, textures, and finishes to meet the needs of any project.

Product Details

AQUALINE SERIES™ PERMEABLE PAVING

Product	Color	Finish	Material	Permeability	Applications
Belgard Permeable Paving	Various	Various	Concrete	Yes	Patios, Driveways, Walkways
Belgard Permeable Paving	Various	Various	Brick	Yes	Patios, Driveways, Walkways
Belgard Permeable Paving	Various	Various	Stone	Yes	Patios, Driveways, Walkways

Learn more about our products and services at www.belgard.com



Product Overview

Our LED light fixtures are designed for long life and energy efficiency. They are available in a variety of sizes and finishes to suit your needs.

Specifications:

- 120V AC, 60Hz, 100W
- 120V AC, 60Hz, 150W
- 120V AC, 60Hz, 200W
- 120V AC, 60Hz, 250W
- 120V AC, 60Hz, 300W
- 120V AC, 60Hz, 350W
- 120V AC, 60Hz, 400W
- 120V AC, 60Hz, 450W
- 120V AC, 60Hz, 500W
- 120V AC, 60Hz, 550W
- 120V AC, 60Hz, 600W
- 120V AC, 60Hz, 650W
- 120V AC, 60Hz, 700W
- 120V AC, 60Hz, 750W
- 120V AC, 60Hz, 800W
- 120V AC, 60Hz, 850W
- 120V AC, 60Hz, 900W
- 120V AC, 60Hz, 950W
- 120V AC, 60Hz, 1000W

Light Fixture Detail

NOT TO SCALE

Signarama

Signarama is a leading provider of outdoor advertising solutions. We offer a wide range of signs, billboards, and digital displays to help you reach your target audience.

Station 22 Parking

Text 122 to: 98000

PLEASE DON'T TAKE TREES TO LEAVE YOUR VEHICLE

Job Name: Charleston Commercial
Dates: 1/18/24
Notes: Station 22 Parking - Zone 122
Reach these sites:

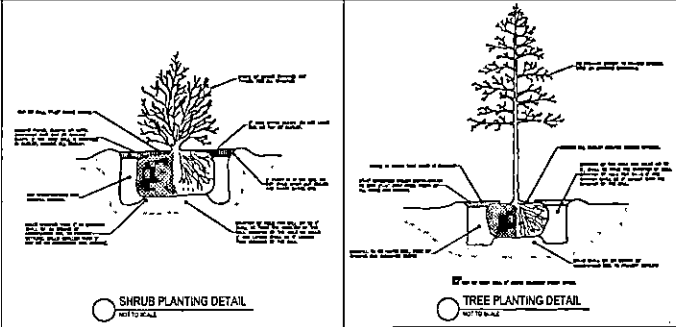
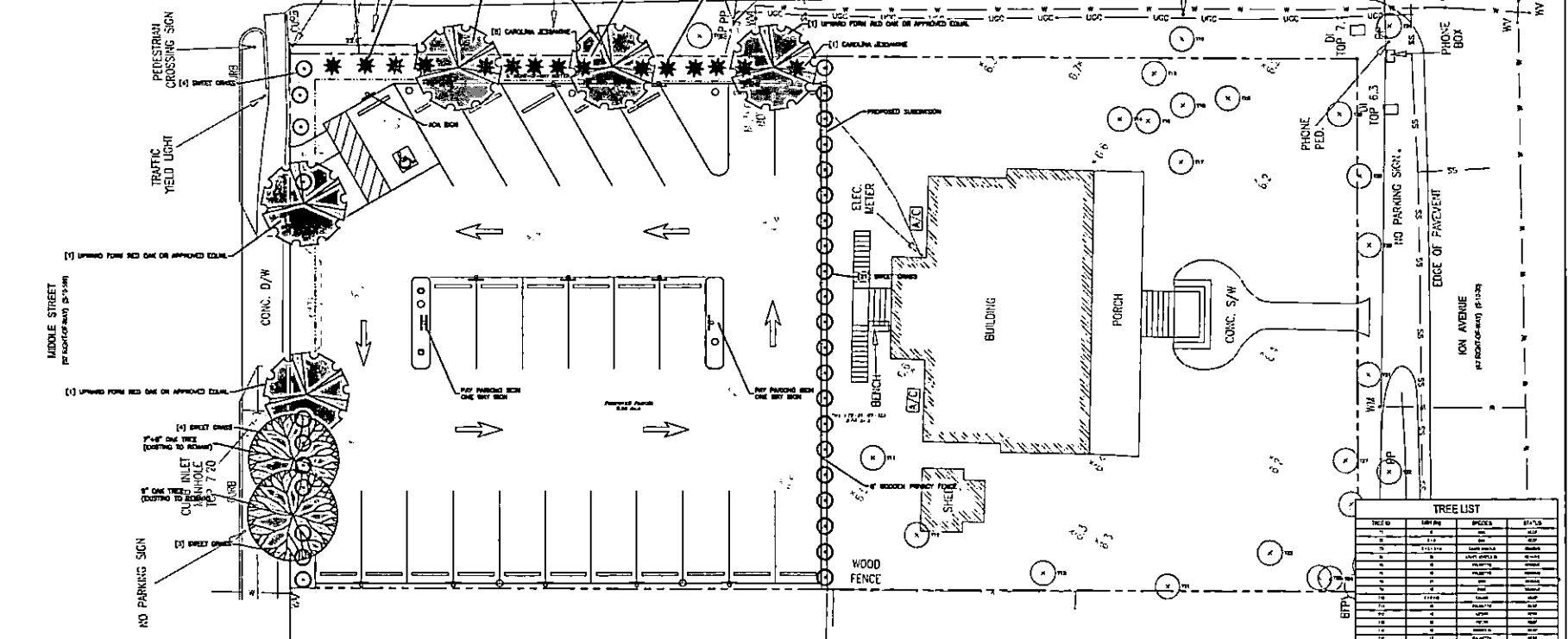
Contractor's APPROVAL:
 Approved
 Rejected with Comments
 Needs not Final Sign
 Other

Signarama's APPROVAL:
 Approved
 Rejected with Comments
 Needs not Final Sign
 Other

Signarama is a leading provider of outdoor advertising solutions. We offer a wide range of signs, billboards, and digital displays to help you reach your target audience.

PLANT AND MATERIAL LIST					
QUANTITY	BOTANICAL NAME	COMMON NAME	SIZE	NOTES	
1	QUERCUS FALCATA	RED OAK	3.0" - 10" IN DIA AT CROWN	DAI	
17	HYDRANGEA CORYMBOSA	SHRUBS	2 GAL		
13	LALUREMIA SPERMATOPHYTES	CANADIAN SPRUCE	2 GAL		
TURFPLANTS					
NOTE: ABOVE QUANTITIES ARE FOR ESTIMATING PURPOSES ONLY. CONTRACTOR WILL MAKE FINAL TAKE OFF.					
NOTE: THE LANDSCAPE INSTALLER SHALL DESIGN AND INSTALL A IRRIGATION SYSTEM THAT WILL SERVE THE NEW PLANTS.					
NOTE: ALL VEGETATION SPECIES ARE SUGGESTED AND MAY BE SUBJECT TO CHANGE TO AN OTHER APPROVED ONE.					

TREE MITIGATION CALCULATION	
TOTAL SHRUBS TO BE REMOVED	17 (2 GAL) - 17% OF TOTAL
TOTAL SHRUBS TO BE REPLACED	17 (2 GAL) - 17% OF TOTAL
TOTAL SHRUBS TO BE REPLACED	17 (2 GAL) - 17% OF TOTAL
TOTAL SHRUBS TO BE REPLACED	17 (2 GAL) - 17% OF TOTAL
TOTAL SHRUBS TO BE REPLACED	17 (2 GAL) - 17% OF TOTAL
TOTAL SHRUBS TO BE REPLACED	17 (2 GAL) - 17% OF TOTAL
TOTAL SHRUBS TO BE REPLACED	17 (2 GAL) - 17% OF TOTAL



1. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS FROM THE CITY OF CHARLOTTE AND THE NORTH CAROLINA DEPARTMENT OF TRANSPORTATION AND HIGHWAYS (NCDOT) PRIOR TO THE START OF WORK.

2. ALL PLANT MATERIAL SHALL BE SUPPLIED BY THE CONTRACTOR. ALL PLANT MATERIAL SHALL BE FIELD CHECKED AND FOUND TO BE IN FULL COMPLIANCE WITH THE SPECIFICATIONS AND QUANTITIES LISTED IN THE PLANT AND MATERIAL LIST. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS FROM THE CITY OF CHARLOTTE AND THE NCDOT PRIOR TO THE START OF WORK.

3. ALL PLANT MATERIAL SHALL BE DELIVERED TO THE SITE IN FULL COMPLIANCE WITH THE SPECIFICATIONS AND QUANTITIES LISTED IN THE PLANT AND MATERIAL LIST. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS FROM THE CITY OF CHARLOTTE AND THE NCDOT PRIOR TO THE START OF WORK.

4. ALL PLANT MATERIAL SHALL BE DELIVERED TO THE SITE IN FULL COMPLIANCE WITH THE SPECIFICATIONS AND QUANTITIES LISTED IN THE PLANT AND MATERIAL LIST. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS FROM THE CITY OF CHARLOTTE AND THE NCDOT PRIOR TO THE START OF WORK.

5. ALL PLANT MATERIAL SHALL BE DELIVERED TO THE SITE IN FULL COMPLIANCE WITH THE SPECIFICATIONS AND QUANTITIES LISTED IN THE PLANT AND MATERIAL LIST. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS FROM THE CITY OF CHARLOTTE AND THE NCDOT PRIOR TO THE START OF WORK.

6. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS FROM THE CITY OF CHARLOTTE AND THE NCDOT PRIOR TO THE START OF WORK.

7. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS FROM THE CITY OF CHARLOTTE AND THE NCDOT PRIOR TO THE START OF WORK.

8. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS FROM THE CITY OF CHARLOTTE AND THE NCDOT PRIOR TO THE START OF WORK.

9. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS FROM THE CITY OF CHARLOTTE AND THE NCDOT PRIOR TO THE START OF WORK.

10. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS FROM THE CITY OF CHARLOTTE AND THE NCDOT PRIOR TO THE START OF WORK.

11. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS FROM THE CITY OF CHARLOTTE AND THE NCDOT PRIOR TO THE START OF WORK.

12. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS FROM THE CITY OF CHARLOTTE AND THE NCDOT PRIOR TO THE START OF WORK.

13. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS FROM THE CITY OF CHARLOTTE AND THE NCDOT PRIOR TO THE START OF WORK.

14. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS FROM THE CITY OF CHARLOTTE AND THE NCDOT PRIOR TO THE START OF WORK.

15. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS FROM THE CITY OF CHARLOTTE AND THE NCDOT PRIOR TO THE START OF WORK.

16. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS FROM THE CITY OF CHARLOTTE AND THE NCDOT PRIOR TO THE START OF WORK.

17. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS FROM THE CITY OF CHARLOTTE AND THE NCDOT PRIOR TO THE START OF WORK.

18. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS FROM THE CITY OF CHARLOTTE AND THE NCDOT PRIOR TO THE START OF WORK.

19. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS FROM THE CITY OF CHARLOTTE AND THE NCDOT PRIOR TO THE START OF WORK.

20. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS FROM THE CITY OF CHARLOTTE AND THE NCDOT PRIOR TO THE START OF WORK.

IRRIGATION MAINTENANCE NOTES:

1. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS FROM THE CITY OF CHARLOTTE AND THE NCDOT PRIOR TO THE START OF WORK.

2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS FROM THE CITY OF CHARLOTTE AND THE NCDOT PRIOR TO THE START OF WORK.

3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS FROM THE CITY OF CHARLOTTE AND THE NCDOT PRIOR TO THE START OF WORK.

4. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS FROM THE CITY OF CHARLOTTE AND THE NCDOT PRIOR TO THE START OF WORK.

5. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS FROM THE CITY OF CHARLOTTE AND THE NCDOT PRIOR TO THE START OF WORK.

TREE LIST					
NO.	DATE	SP.	TR.	DBH	HGT.
1	1/15/10	100	100	100	100
2	1/15/10	100	100	100	100
3	1/15/10	100	100	100	100
4	1/15/10	100	100	100	100
5	1/15/10	100	100	100	100
6	1/15/10	100	100	100	100
7	1/15/10	100	100	100	100
8	1/15/10	100	100	100	100
9	1/15/10	100	100	100	100
10	1/15/10	100	100	100	100

DATE	DESCRIPTION

CLINE ENGINEERING

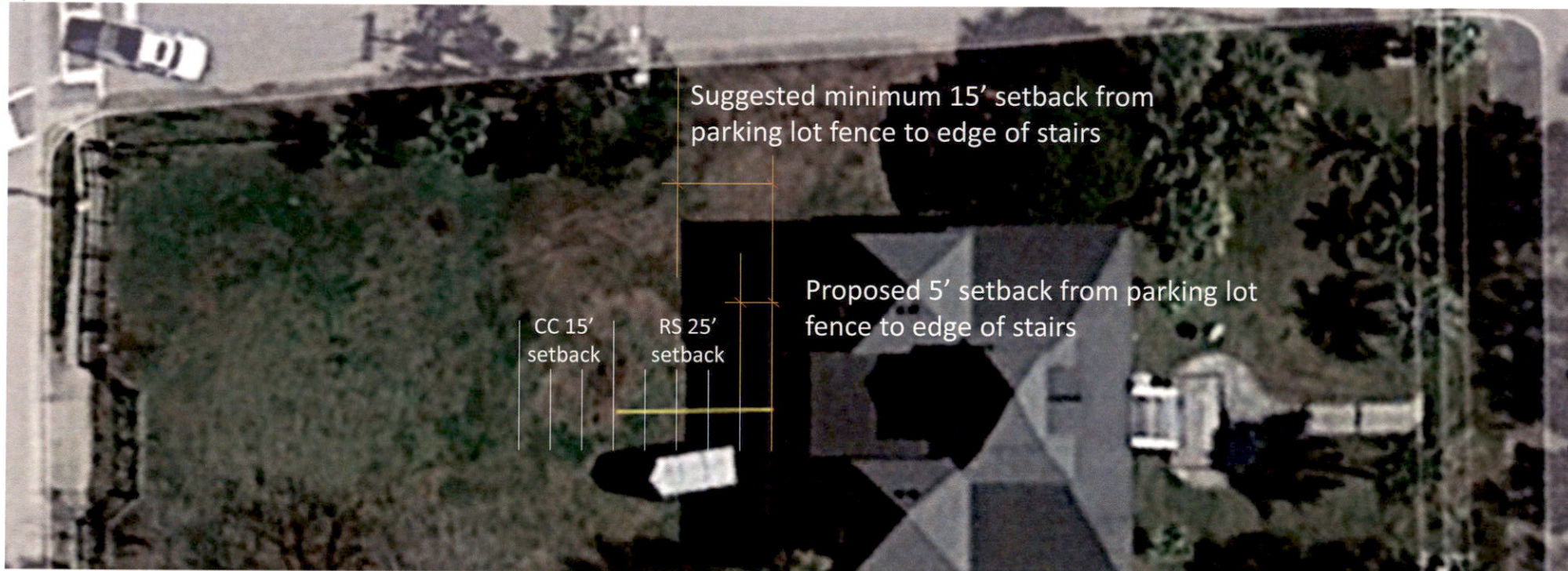
PROFESSIONAL DESIGN CONSULTING

SITE PLAN

INTERSECTION OF MIDDLE STREET AND STATION 22 STREET
BULLYBANK ISLAND, SOUTH CAROLINA

SHEET NUMBER: L-1

Exhibit
2



ARTICLE III. RS-Single Family Residential District.

Sec. 21-22. Front, side and rear setbacks.

E. Minimum rear yard setback. (1) Unless otherwise provided, no structure or part of a structure shall be erected or allowed to remain nearer than twenty-five (25) feet to a Rear Lot Line. (2) Shallow lot: (a) A Shallow Lot is a lot with less depth than width at the Building Line. (b) The required Rear Yard Setback may be reduced by one-third (1/3) foot for each foot by which the lot depth is less than eighty (80) feet rounded up or down to the nearest whole foot; provided, however, the Rear Yard Setback shall never be less than ten (10) feet. (c) This provision does not eliminate the required Setbacks from the RC-1 and RC-2 Districts stipulated in article III 21-23. (3) See required Setbacks from RC-1 and RC-2 Districts in article III 21-23. (4) See required Setbacks for Accessory Structures in ARTICLE XV.

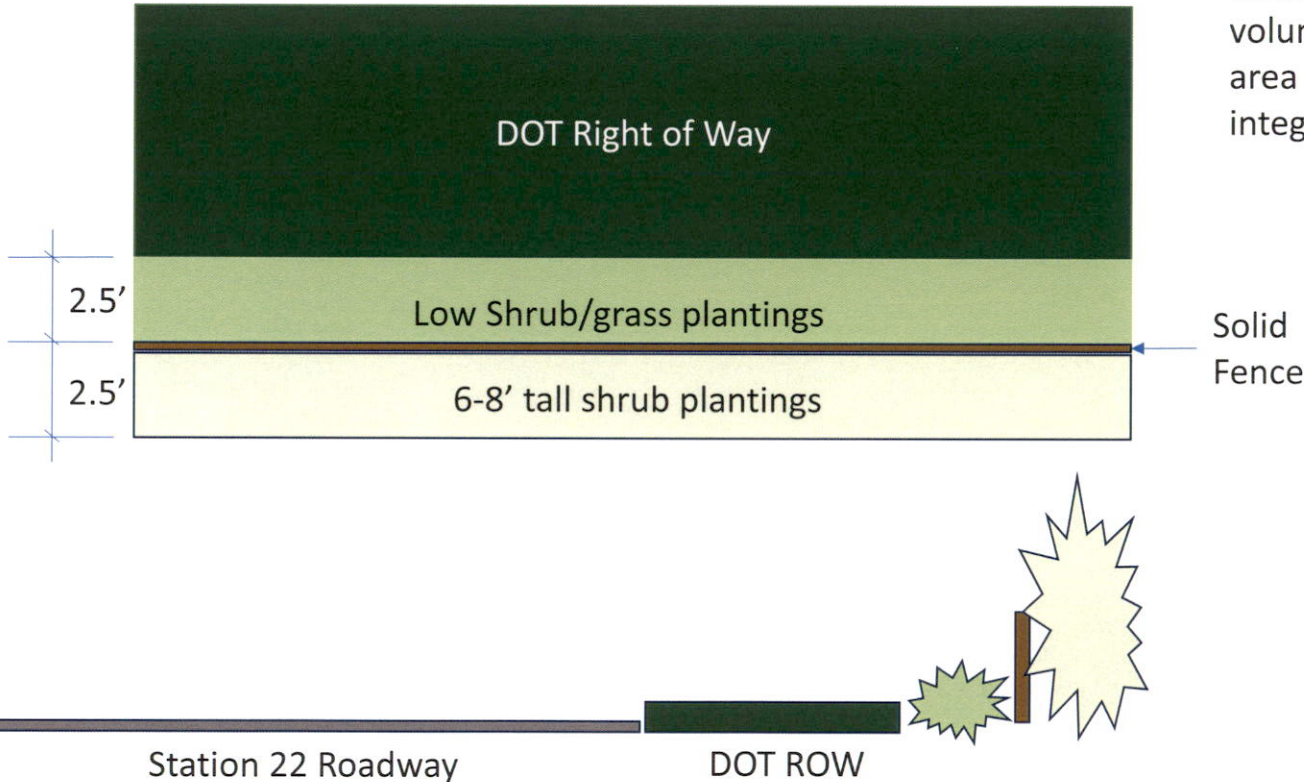
ARTICLE IV. CC-Community Commercial District.

Sec. 21-53. Front, side and rear setbacks

D. Minimum rear yard setback. (1) The minimum Rear Yard Setback from the adjoining residentially zoned portion of the lot shall be fifteen (15) feet.

2

Station 22 Roadway



Station 22 is the “entrance” to the residential district from the commercial area, and a high-volume walking area for Island visitors. The area should be attractive to maintain the integrity of the residential neighborhood



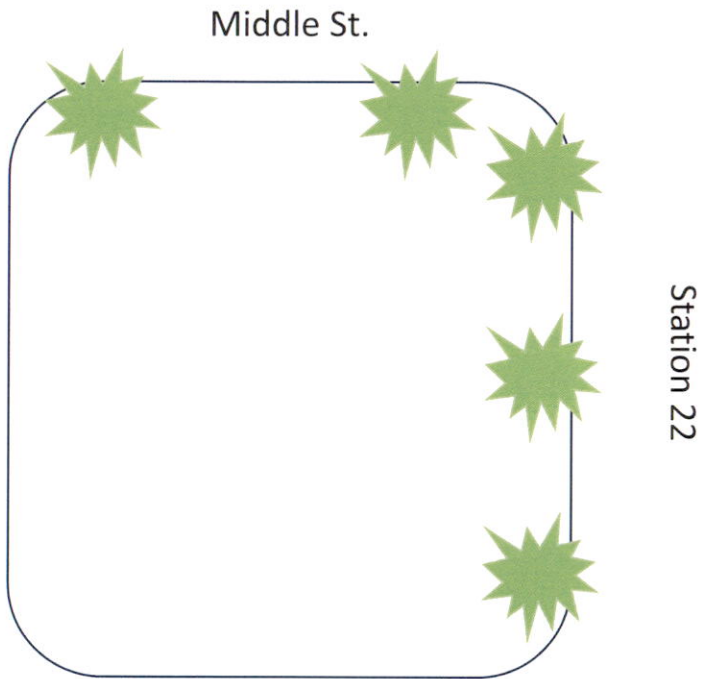
Sec. 21-143. Parking and outdoor storage.

D. Parking requirements on Split zoned lots within the CC-Community Commercial District and on commercial lots within the CCOD 1 and CCOD 2 Districts. (3-20-12)

(4) Buffer and landscape plan for parking lot. The applicant shall submit a proposed buffer and landscape plan to the Design Review Board for their approval. At a minimum, each parking newly developed or redeveloped commercial lot area shall have a six (6) to eight (8) foot fence high solid fence or wall abutting the residential zoning district boundary; lot area adjacent to the right-of-way lines shall be planted borders not less than five (5) feet in width parallel to the right-of-way (except where egress to the street is needed). The planted border areas shall have at least one (1) tree (having a minimum clear trunk of five (5) feet and a minimum height of eight (8) feet for each thirty (30) linear feet of the border area). T

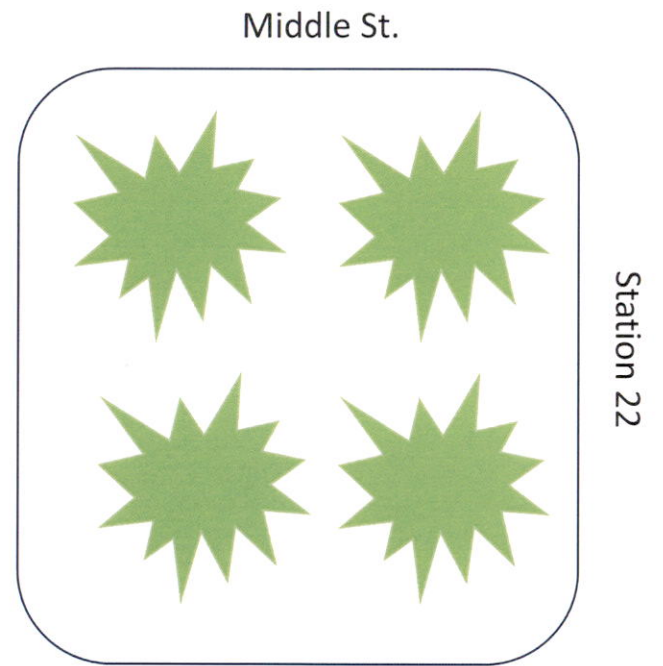
3

As planned



Suggest large canopy species trees interspersed within the parking area in lieu of trees in border area @ 30' spacing to provide a more natural appearance

VS





Current



Louvered





JETTIES
PERMIT & CLIENT PARKING
POE'S PATRON PARKING
MONDAY - FRIDAY 8PM - 12AM
SATURDAY & SUNDAY 10PM - 12AM
PARKING ENFORCED - VIOLATORS WILL BE BOITED
BY HONOLULU MUNICIPALITY - 848.3623.1400

The
JETTIES
HONOLULU ISLAND
PERMIT & CLIENT PARKING
POE'S PATRON PARKING
MONDAY - FRIDAY 8PM - 12AM
SATURDAY & SUNDAY 10PM - 12AM
PARKING ENFORCED - VIOLATORS WILL BE BOITED
BY HONOLULU MUNICIPALITY - 848.3623.1400

TOWN OF SULLIVAN'S ISLAND
Design Review Board Meeting
February 20 2024, 4:00 p.m.
PUBLIC INPUT SIGN UP SHEET

****PLEASE SIGN THIS FORM IF YOU WOULD LIKE
 TO SPEAK ON A SPECIFIC AGENDA ITEM****

NAME	AGENDA ITEM
(Rev) Lawrence McINERNEY	1118 Osceola (Stella Maria Church)
Barbara Flaherty	2118 Parking Lot on middle
GARY Visser	Stella Maria
George Detyens	Stella Maria
Randy Wilgis	PARKING LOT 2118 100N AV
Kimberly Brown	2118 Ion
Joel Traubham	2513 Ion 1118 Osceola.
David Russell	1118 Osceola
Cyndy Ewing	2514 Ion Ave.
Ian Watterson	2620 Atlantic